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Tuesday, 25 July 2023

Dear Sir/Madam

OVERVIEW & SCRUTINY COMMITTEE

A meeting of the Overview & Scrutiny Committee has been arranged to take place **WEDNESDAY, 2ND AUGUST, 2023 at 6.00 PM IN THE COMMITTEE ROOM** District Council House, Lichfield to consider the following business.

Access to the Committee Room is via the Members' Entrance.

The meeting will be live streamed on the Council's [YouTube channel](#)

Yours faithfully

A handwritten signature in cursive script that reads 'Kerry Dove'.

Kerry Dove
Chief Operating Officer

To: Members of Overview & Scrutiny Committee

Councillors Norman (Chair), Leung (Vice-Chair), Ball, Booker, Ho, Hawkins, Hill, Holland, Trent, Ray, Robertson, Whitehouse, Woodward and B Yeates



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AGENDA

1. Apologies for Absence
2. Declarations of Interests
3. Minutes of the Previous Meeting 3 - 6
4. **Health Matters** 7 - 12

To receive updates of the work of the County Council's Health and Care Overview & Scrutiny Committee, feedback issues via our representative on that Committee and consider any health related matters devolved to us by that Committee. The work programme is attached to aid Members.
5. City Centre Pedestrianisation Trial 13 - 164
6. Review of the Overview & Scrutiny function at the Council 165 - 182
7. Local Elections 2023 Review 183 - 186
8. Work Programme 187 - 188
9. **EXCLUSION OF PRESS AND PUBLIC**

RESOLVED: "That as publicity would be prejudicial to the public interest by reason of the confidential nature of the business to be transacted, the public and press be excluded from the meeting for the following items of business, which would involve the likely disclosure of exempt information as defined in Paragraph 1 of Part 1 of Schedule 12A of the Local Government Act 1972".

IN PRIVATE

10. Verbal Update - Cinema Update



OVERVIEW & SCRUTINY COMMITTEE

8 JUNE 2023

PRESENT:

Councillors Norman (Chair), Leung (Vice-Chair), Ball, Booker, Ho, Hill, Trent, Whitehouse, Woodward and B Yeates

1 APOLOGIES FOR ABSENCE

Apologies for absence were received from Councillors Holland, Ray and Robertson.

The Chair thanked the previous chair of the Committee, Councillor M. Wilcox for his work in the role over the past year.

2 DECLARATIONS OF INTERESTS

There were no declarations of interests.

The Chair reminded the Committee that Overview & Scrutiny should not be subject to party whips. It was noted that the Committee should consider items for the whole district.

3 MINUTES OF THE PREVIOUS MEETING

The minutes of the previous meeting were agreed as a correct record.

4 TERMS OF REFERENCE

The terms of reference as stated in the Council's Constitution were considered by the Committee. Members were content with the terms and noted them. It was agreed that the Terms of Reference would be helpful to refer to at the forthcoming review of the Overview & Scrutiny function at the Council following the change to a one Committee system.

5 TRAINING FOR MEMBERS

The Chair introduced the Committee to the Overview & Scrutiny Toolkit which outlined the objectives and process to carry out the O&S function at Lichfield District Council.

There was focus discussion on how all Members of the Council could request a subject matter be considered and investigated by the Committee. It was noted that once a request was received, the Chair and Vice Chair would consider it against criteria including link to the Council's Strategic Plan and impact the subject has on the District.

There was suggestion and it was agreed to look at training for Committee Members on areas such as questioning skills.

RESOLVED: That training providers for Overview & Scrutiny skills be sourced.

6 WORK PROGRAMME

The Committee discussed the work programme. It was reported that it currently showed items carried over from the previous Council term or already known to be required, for example the Medium Term Financial Strategy, had been listed and Officers were awaiting the Committee's views and instructions to add more.

The Committee heard a request from the Cabinet Member for Finance and Commissioning to have a special meeting in December to help bring forward items in the forthcoming budget. It was agreed that this worked well in the previous year and would aid a more consensual and transparent approach to setting the budget and ensure equality of funding across the District. It was noted that a date had been set aside however it was noted that it clashed with a City Council meeting and so it was agreed for Officers to find a new date to be circulated to Members.

When asked, it was confirmed that a review of the Civic function and matrix had been requested as the type events attended by the Chair of Council had changed since Covid but also there had been a request by the Independent Remuneration Panel for the Council to review the expenses scheme, It was agreed that this had to be dealt with, though it was not considered a priority, that Overview and Scrutiny was the right place and would not require a lengthy report at a future meeting.

Task Groups were discussed and it was agreed that there will be notes of meetings due at every O&S Committee meeting. It was also reported that any recommendation from a task group would be presented to the full Committee for consideration. It was noted that task groups should also aim to be as transparent in their work as possible and when confidential matters are required to be discussed, the usual process for this is followed. It was agreed to review all current task groups to ensure their remit and membership is still relevant.

It was requested by the Committee that the following items be added.

- City Centre Pedestrianisation Trial
- Empty Homes Policy
- Funding for Infrastructure in Burntwood and rural areas
- Update on the Cinema project
- Review of the Overview & Scrutiny function at the Council

It was noted that the Forward Plan was a guide to help Committee Members formulate the work programme. It was reported however that many items on the Forward Plan were the decisions due at the end of the process as a sign off and so the O&S Committee to look at everything on the Forward Plan would not be an efficient use of their time. It was requested by the Leader of the Council for the Committee to notify Cabinet of what they wish to investigate or be part of the policy's development.

RESOLVED: That the process of setting the work programme be noted and the agreed items be added.

7 PLANNING PROTOCOL

This item was deferred to the 14th of September meeting which will have given new members of the Planning Committee time to be able express a considered view of any changes proposed.

(The Meeting closed at 7.18 pm)

CHAIRMAN

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Health and Care Overview and Scrutiny Committee Draft Work Programme 2023/24

This document sets out the work programme for the Health and Care Overview and Scrutiny Committee for 2023/24.

The Health and Care Overview and Scrutiny Committee is responsible for:

- Scrutiny of matters relating to the planning, provision and operation of health services in the Authority's area, including public health, in accordance with regulations made under the Health and Social Care Act 2001 and subsequent guidance.
- Scrutiny of the Council's work to achieve its priorities that Staffordshire is a place where people live longer, healthier and fulfilling lives and In Staffordshire's communities people are able to live independent and safe lives, supported where this is required (adults).

Link to Council's Strategic Plan Outcomes and Priorities

- Inspire healthy, independent living
- Support more families and children to look after themselves, stay safe and well

We review our work programme at every meeting. Our focus in scrutiny is on tangible outcomes for the residents of Staffordshire, to use the data provided and members experience to debate and question the evidence, to provide assurance in what is being done and reassurance that matters within the health and care system are moving in the right direction. Scrutiny of an issue may result in recommendations for NHS organisations in the county, the County Council and for other organisations.

To review our meetings they can be found on this link: [Browse meetings - Health and Care Overview and Scrutiny Committee](#)

Councillor Jeremy Pert

Chairman of the Health and Care Overview and Scrutiny Committee

Health and Care Overview and Scrutiny Committee Work Programme 2023-24

Date	Topic	Background/Outcomes	
Committee Meetings, Reviews and Consultations To review meetings they can be found on this link: Browse meetings			
	Topic	Background/ Basis	Actions/ Outcomes
Monday 12 June 2023 at 10.00 am Completed	<ul style="list-style-type: none"> • Primary Care Dental Overview • Primary Care Access • Primary Care Estate • Work Programme 2023-24 	Reports as identified in the Work Programme Annual update of Work Programme	<ul style="list-style-type: none"> • The Committee receive a briefing on the delivery of orthodontics in Staffordshire. • The Committee write to the ICB and Keele university to support a dental school at Keele University. • The Committee receive a briefing note on the model for assessing new development sites. • The Committee congratulated Midlands Partnership Foundation Trust on gaining University Hospital status. • The membership of the Women’s Health Strategy Working Group be updated outside of the meeting and be reported back at the next meeting. <ul style="list-style-type: none"> ○ Janice Silvester-Hall ○ Ann Edgeller ○ Monica Holton ○ Jill Hood ○ Val Chapman • The membership of the Integrated Care Hubs Working Group be updated outside of the meeting and be reported back at the next meeting. <ul style="list-style-type: none"> ○ Richard Cox ○ John Jones (SMDC) ○ Barbara Hughes (SMDC) ○ Lyn Swindlehurst (SMDC) ○ Linda Malyon (SMDC) ○ Dave Jones (NULBC) ○ Ian Wilkes (NULBC) ○ Rupert Adcock (NULBC) ○ Gill Heesom (NULBC)

Thursday 6 July 2023 at 4:30pm Health and Care Training Session	<ul style="list-style-type: none"> Health and Care training delivered by Centre for Governance and Scrutiny 		<ul style="list-style-type: none"> Centre for Governance and Scrutiny provided a training session for Health and Care O&S on upcoming changes in legislation. A link to the training session will be made available.
Monday 24 July 2023 at 10.00 am Scheduled	<ul style="list-style-type: none"> ICP Operating Plan System performance System Pressures Update on Elective care performance and recovery SSOT ICS People, Culture and Inclusion Annual Report and update. 		
Monday 31 July 2023 Scheduled	<ul style="list-style-type: none"> Introduction to Adult Social Care Assurance 	To review Social Care Services and provide assurance	
Summer (date tbc)	<ul style="list-style-type: none"> Member workshop to assess access to information on Social Care 		
Wednesday 6 September 2023 at 10:00 – Site visit tbc	<ul style="list-style-type: none"> MPFT NSCHCT 	Site visit to view community-based services	
Monday 11 September 2023 at 10.00 am Scheduled	<ul style="list-style-type: none"> MPFT – Community Based health update NSCHCT – Community Based health update Joint mental health & mental wellbeing strategy: “good mental health in Staffordshire” 2023/28 action plan Mental Health in Schools update. 	Community based health update from MPFT and NSCHCT and feedback from site visit	

Monday 2 October at 10:00am	<ul style="list-style-type: none"> • Possible enquiry session re social care education (requested March 2023) 	Agreed by Committee 24 March 2023	
Monday 16 October 2023 at 10.00 am Scheduled	<ul style="list-style-type: none"> • ASC Workforce • ICS Workforce • Maternity Services • 1st year of life (Public Health) • Feedback from Social Care education session 	Agreed by Committee 24 March 2023	
Autumn (date tbc)	<ul style="list-style-type: none"> • Member workshop to assess access to financial assessments 		
Monday 27 November 2023 at 10.00 am Scheduled	<ul style="list-style-type: none"> • Social Prescribing • Feedback from two workshops • General Practice Update • Dentistry 	Review impact on investment on Social prescribing Agreed by Committee 12 June 2023 Agreed by Committee 12 June 2023	
Monday 29 January 2024 at 10.00 am Scheduled			
Monday 18 March 2024 at 10.00 am Scheduled			

Work programme for 2023-24 - items		Background	Target Scheduling Date
Briefings	<ul style="list-style-type: none"> • Quality Accounts NHS Trusts 	Annual	May/June 2023
	<ul style="list-style-type: none"> • Care market 		
	<ul style="list-style-type: none"> • Public Health <ul style="list-style-type: none"> ○ PH Annual Report ○ PH Dashboard ○ Developing Healthier Communities 		

	updates		
Requested in 2022-23	<ul style="list-style-type: none"> • Impact of air pollution on health • Impact of Long COVID • Obesity and Diabetes • End of Life – compassionate communities (working group?) • Innovation / technology – JJ ASC, JP NHS (Phillipa Haden) • Health Visitor Service • Healthwatch Annual Report 		

<p>Membership Jeremy Pert (Chair) Richard Cox (Vice-Chair - Overview) Ann Edgeller (Vice-Chair - Scrutiny) Charlotte Atkins Philip Atkins Keith Flunder Thomas Jay Phil Hewitt Jill Hood Bernard Peters Janice Silvester-Hall Mike Sutherland Ian Wilkes</p> <p>Borough/District Councillors Ann Edgeller (Stafford) David Williams (Cannock Chase) Monica Holton (East Staffordshire) Leona Leung (Lichfield) Ian Wilkes (Newcastle-under-Lyme) Val Chapman (South Staffordshire) John Jones (Staffordshire Moorlands) Chris Bain (Tamworth)</p>	<p>Calendar of Committee Meetings at County Buildings, Martin Street, Stafford. ST16 2LH (at 10.00 am unless otherwise stated)</p> <p>Monday 12 June 2023 at 10:00 am Monday 24 July 2023 at 10.00 am; Monday 31 July 2023 at 10.00 am; Monday 18 September 2023 at 10.00 am; Monday 16 October 2023 at 10.00 am; Monday 27 November 2023 at 10.00 am; Monday 29 January 2024 at 10.00 am; Monday 18 March 2024 at 10.00 am;</p>
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Lichfield City Pedestrianisation

Cllr Janice Silvester-Hall

Date:	2 August 2023
Agenda Item:	Pedestrianisation
Contact Officer:	Martin Gritt
Tel Number:	01543 308053
Email:	martin.gritt@lichfielddc.gov.uk
Key Decision?	No
Local Ward Members	



Overview & Scrutiny

1. Executive Summary

- 1.1 This report provides an update on the implementation of an Experimental Traffic Regulation Order, over a trial period of 18 months in Lichfield City Centre.
- 1.2 A Traffic Regulation Order is a legal order, which allows the Local Highway Authority to regulate the speed, movement and parking of vehicles. The Act governing Traffic Orders is the Road Traffic Regulation Act 1984, and this is enforceable by law. In Staffordshire, moving Traffic Orders are enforced by the police and on-street parking restrictions are enforced by Staffordshire County Council (SCC). An Experimental Traffic Regulation Order is very similar to a Traffic Regulation Order except that the Order is not permanent. Instead, an Experimental Traffic Regulation Order can only be in place for a maximum of 18 months. Experimental Traffic Regulation Orders are often used by Local Highway Authorities to measure and assess the effects of new arrangements, before potentially making them permanent.
- 1.3 As part of the Experimental Traffic Regulation Order, a six-month formal consultation was launched so that feedback and objections from all users/stakeholders can be recorded and formally considered.

2. Recommendations

- 2.1 That the committee note the contents of the report.
- 2.2 That the Overview and Scrutiny Committee will provide feedback on the trial to date, and that the results of the consultation process will be reported to the committee prior to any potential changes to make pedestrianisation permanent.

3. Background

- 3.1 On 9 November 2021 Lichfield District Council approved its Public Realm Strategy at **Appendix A**. Whilst that strategy did not consider a scheme in the shape of what became the Experimental Traffic Regulation Order, it did set out the high value Lichfield District Council placed on active modes of travel and making the city centre more attractive to shoppers, workers, and visitors.
- 3.2 An Experimental Traffic Regulation Order is similar to a Traffic Regulation Order, except that the order is not permanent. Instead, an Experimental Traffic Regulation Order can only be in place for a maximum of 18 months. Experimental Traffic Regulation Orders are often used by local highway authorities (in this case Staffordshire County Council) to assess the effects of new arrangements, before potentially making them permanent. The Experimental Traffic Regulation Order includes a consultation period and for the first six months a consultation period is launched so that feedback and objections from all users/stakeholders can be recorded and formally considered.

- 3.3 A city centre pedestrianisation report by Amey Consulting for Staffordshire County Council was produced in August 2021 at **Appendix B**. The report sets out the detail of the previous pedestrianised zones, which permitted access for loading, disabled badge holders and permit holders leading to an average daily flow of 550 vehicles. It was noted that traffic movements of this magnitude ‘would not be associated with a pedestrian zone as they restrict pedestrian access’.
- 3.4 The Amey report then considered options for the pedestrianisation of various roads in the city centre, going beyond what was already in place.

These options were:

- a) Option 1: introducing additional restrictions during peak pedestrian activity to prohibit all motorised vehicles between 10am and 4pm. The cost was estimated at £30,000.
- b) Option 2: closing certain streets to disabled badge holders but not loading and permit holders. The estimated cost was £500,000.
- c) Option 3: removing all disabled parking from the pedestrian zone, with the pedestrian zone extended to at any time, albeit with access for loading remaining. It was proposed that more detailed consideration of suitable locations for disabled parking would need to be undertaken. The estimated cost was £20,000.
- d) Option 4: in addition to the above proposals were made in respect of the Bird Street one way system.

The report made recommendations that ‘options 3 and 4 offer the maximum benefit to pedestrians and positive change to the pedestrian zone environment’. This was because:

Traffic survey data indicates that the approximately 80% of vehicles within the pedestrian zone are private cars therefore removal of the need to access the zone by removing disabled parking and reversal of the Bird Street one way system will have the greatest effect to traffic flows by removing the reason for vehicles to enter.

- 3.5 As part of progressing the proposed scheme, a report on moving Blue Badge parking bays was commissioned by Lichfield District Council, appointing AccessAble. Following an initial report commissioned in July 2020 (as such a move of bays was initially a response to Covid-19) a further report on making the change permanent was produced dated October 2022 at **Appendix C**. A number of recommendations were included within the AccessAble report, an update can be found at **Appendix D**. The District Council also announced that Blue Badge holders can park in any council owned carpark free of charge, providing a valid Blue Badge is present. Blue Badge holders may also park on single or double yellow lines for up to 3 hours, but in general not where there are restrictions on loading or unloading – indicated by yellow kerb dashes and / or signs on plates.
- 3.6 A Cabinet Member Report was produced dated 3 November 2022 at **Appendix E**.
- 3.7 A Cabinet Member Decision was made to progress the ETRO on 10 November 2022, the reasons given were:
- Implementing the ETRO with the aim to make Lichfield city centre even more attractive to shoppers, workers, and visitors, to reduce traffic movement and reduce air pollution in this locality whilst giving priority to pedestrians where appropriate.*
- The ETRO restrictions would prohibit motor vehicles travelling through the following city streets at all times: Market Street, Tamworth Street, Conduit Street, Breadmarket Street, Bore Street, Dam Street, Sandford Street, and Bird Street, except for loading between the hours of 21:00 and 12:00 the following day, and no waiting at any time.
- 3.8 Informal consultation began from January 2023, with a drop-in event held at the Guildhall on 17 January 2023 & 1 March 2023.
- 3.9 The ETRO was made on 2 March 2023 (**Appendix F**). The following reason was given for the ETRO:

To ensure a continuous and standard approach to traffic movement and parking control across the retail centre of the city. To improve safety for pedestrians and simplify the existing orders to standard times.

3.10 Formal consultation started from March 2023 so that feedback and objections from all users/stakeholders can be recorded and formally considered.

SCC Pedestrianisation Survey:

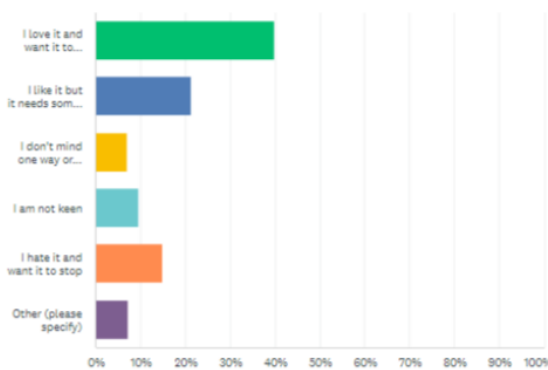
To date, the Pedestrianisation Feedback Survey launched by Staffordshire County Council is still ongoing, with 272 responses received to date: 64 in favour and 208 against.

LDC Pedestrianisation Survey:

To date, the Pedestrianisation Feedback Survey launched by Lichfield District Council is still ongoing, with 2,108 responses received to date. Further updates and analysis will be provided once the survey is closed.

How would you describe your feelings about the pedestrianisation of the city centre?

Answered: 2,108 Skipped: 3



ANSWER CHOICES	RESPONSES	
I love it and want it to continue	39.94%	842
I like it but it needs some amendments	21.30%	449
I don't mind one way or another	7.07%	149
I am not keen	9.68%	204
I hate it and want it to stop	14.85%	313
Other (please specify)	Responses 7.16%	151
TOTAL		2,108

Engagement Activity:

Since January 2023, various engagement activities were undertaken to involve visitors, residents and businesses in the decision-making process. These activities included social media campaigns on Facebook, Twitter, and LinkedIn, press releases, posters, and distribution of e-bulletins to residents and businesses.

Resident Bulletins:

Several bulletins were sent to residents, providing them with information and updates related to the trial pedestrianisation.

Printed Collateral:

Printed collateral was also used to disseminate information and engage residents and businesses. These included posters in businesses and business windows close to the pedestrianisation zone, also in the Guildhall and at Lichfield District Council House.

Press Releases:

Press releases to all local and regional press and media contacts were issued throughout the process before every survey was launched and at key moments of change.

Summary:

The engagement activities conducted since January 2023 have been extensive and varied. The Pavement Café Survey, social media campaigns, press releases, posters, in-person handouts and e-bulletins were used to actively involve residents and businesses in the trial pedestrianisation process. The information collected from these engagement efforts will contribute to informed decision-making regarding the trial pedestrianisation in Lichfield city centre. Full summary of engagement, including analysis of the survey responses received is available at **Appendix G**.

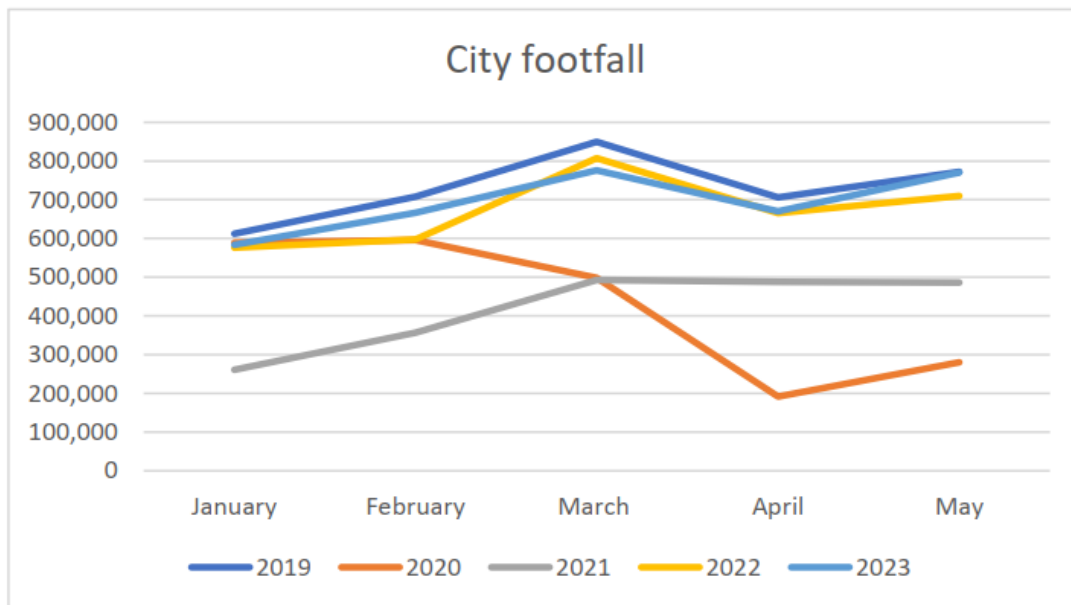
- 3.12 A workshop event was held on 17 April 2023, during the sessions, we asked people to share their ideas of how the current Experimental Traffic Regulation Order could be altered, amended, or improved. We also asked people to share their concerns about the trial with us. Full feedback is detailed here: [City centre pedestrianisation trial \(ETRO\) – April consultation event views and comments \(lichfielddc.gov.uk\)](https://www.lichfielddc.gov.uk/city-centre-pedestrianisation-trial-et-ro-april-consultation-event-views-and-comments)
- 3.13 A follow up workshop event was held on 10 July 2023. This event was an opportunity to provide an update on pedestrianisation and share the proposal based on feedback received. The proposal received mixed views. Full feedback is detailed here: [City centre pedestrianisation trial \(ETRO\) – July workshop events views and comments \(lichfielddc.gov.uk\)](https://www.lichfielddc.gov.uk/city-centre-pedestrianisation-trial-et-ro-july-workshop-events-views-and-comments)
- 3.14 As a result of feedback received via consultation, the proposal is to introduce a hybrid pedestrianisation. If introduced, the proposal would see part-time pedestrianisation:
- o Access allowed to blue badge holders/permit holders on Mondays, Tuesdays, and Wednesdays at any time.
 - o Thursdays, Fridays, Saturdays, and Sundays will be pedestrianised as per the current trial (A prohibition on motor vehicles at all times, except for loading between the hours of 21:00 and 12:00 the following day, and no waiting at any time).

If this is to be introduced, a new Experimental TRO would be implemented with a new six-month consultation period so that feedback and objections from all users/stakeholders can be recorded and formally considered as part of the revised scheme.

- 3.15 The Monday – Wednesday proposal has also been shared with a number of businesses who attended a business specific workshop, the responses were favourable with the majority of those represented at the workshop would welcome the change.
- 3.16 Footfall within the city centre continues to be monitored. The following statistics are taken from footfall counters that measure footfall in Tamworth Street, Dam Street, Market Street and Bird Street.
- Whilst the statistics show that the footfall figures in the city are not yet back to pre-covid levels, it does show that they are recovering in line with pre-covid levels – as illustrated by the two blue lines with the darker blue being pre-covid and the light blue being current figures. It shows that in May 2023 the footfall was just 1,926 below pre-covid levels.

The figures also show that in March 2023 the figure dipped significantly below 2022's footfall figures, however as this did not continue as a trend in April or May, it is hard to attribute this to any one issue. The figures are however being kept under review ongoing.

Month	2019	2020	2021	2022	2023
January	612,253	588,705	260,937	576,315	583,673
February	707,752	595,871	357,042	597,218	666,741
March	849,707	498,686	492,755	807,467	775,977
April	705,966	191,717	487,711	665,961	670,605
May	772,121	279,895	485,718	710,161	770,195



Next Steps

- 3.17 As a result of feedback via consultation, the Cabinet Member for High Streets & Visitor Economy has introduced the following changes:
- o Access allowed to blue badge holders/permit holders on Mondays, Tuesdays, and Wednesdays at any time.
 - o Thursdays, Fridays, Saturdays, and Sundays will be pedestrianised as per the current trial (A prohibition on motor vehicles at all times, except for loading between the hours of 21:00 and 12:00 the following day, and no waiting at any time).
- 3.18 The amended E TRO will be implemented with a new six-month consultation period so that feedback and objections from all users/stakeholders can be recorded and formally considered as part of the revised scheme.
- 3.19 Enforcement of the restrictions will be introduced once Staffordshire County Council have amended the E TRO and installed the signage.
- 3.20 Notice of the changes and an update on enforcement will be issued via all available methods of communications.

Alternative Options	<ol style="list-style-type: none"> 1. Continue with current restrictions and continue to engage with stakeholders to determine an alternative proposal. 2. To stop the Experimental TRO process and revert back to the original 'part pedestrianised' scheme within the city centre.
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<p>Consultation</p>	<ol style="list-style-type: none"> 1. We have consulted with AccessAble on this decision to ensure that the council has considered suitable alternative arrangements and the needs of disabled Blue Badge holders. 2. Lichfield City Council have confirmed their in-principal support for a pedestrianised City Centre, pending further discussion and analysis of a detailed scheme proposal and its implications 3. The council's city centre masterplan Member Task Group are supportive of a pedestrianised city centre. 4. Consultation with members of the public and businesses has been on-going, by continuously assessing the trial's impact, we aim to enhance the scheme's effectiveness and contribute to the creation of a safe, prosperous and vibrant city centre.
<p>Financial Implications</p>	<ol style="list-style-type: none"> 1. Majority of revenue through Major Projects budget (£22,500.00), and an additional capital sum (£42,284.00) from S106 allocated to the project: <ul style="list-style-type: none"> • Consultant fees (Amey, AccessAble, Legal advice): £49,816.35 • Improvements (New bays, benches, signage): £1,932.94 • Communications (Printed material, venue hire): £574.86 2. Project is within budget and do not expect to exceed budget. 3. There will be an additional spend of circa £6,000.00 to implement the changes to the E TRO.
<p>Approved by Section 151 Officer</p>	<p>Yes</p>
<p>Legal Implications</p>	<ol style="list-style-type: none"> 1. The council appointed DWF to advise on the legal process for completing the ETRO to ensure that the legislation is being adhered to.
<p>Approved by Monitoring Officer</p>	<p>Yes</p>
<p>Contribution to the Delivery of the Strategic Plan</p>	<ol style="list-style-type: none"> 1. Restricting vehicle access in the city centre supports the priority of shaping place and developing prosperity. 2. Enabling people to live healthy and active lives by allowing more pedestrianisation space within the city centre. 3. Develop prosperity to encourage economic growth by supporting businesses with their requests for pavement licenses for outdoor space.

<p>Equality, Diversity and Human Rights Implications</p>	<ol style="list-style-type: none"> 1. In introducing the removal of the Disabled Persons Parking Bays, the District Council engaged with an advisory body, AccessAble to determine the impact on Blue Badge holders of the proposal. 2. The Council has dedicated provision for alternative parking (with no net loss), to meet the needs of Blue Badge holders, these being facilities within Bird Street Car Park and Lombard Street Car Park both located near shops, cafes and other facilities. 3. Blue Badge holders can also park in any bay on any council car park free of charge if there are no Disabled Persons Parking Bays available when displaying their Blue Badge. 4. Blue badge holders can also park legally on single or double yellow lines within the city for up to three hours if they are not causing an obstruction. 5. This dedicated provision will continue to be kept for Blue Badge holders whilst the ETRO is implemented. 6. An Equality Impact Assessment has been carried out and continues to be updated throughout the Experimental Traffic Regulation Order.
<p>EIA (Equality Impact Assessment) logged by Equalities Officer</p>	<p>Yes. The EIA will continue to be updated throughout the trial with the latest version available at: City centre pedestrianisation trial (ETRO) – Key documents (lichfielddc.gov.uk)</p>
<p>Crime & Safety Issues</p>	<ol style="list-style-type: none"> 1.

<p>Environmental Impact (including Climate Change and Biodiversity).</p>	<ol style="list-style-type: none"> 1. The reduction in vehicles in the city centre will see a change in the character and appearance of those streets where cars would have previously driven/parked. 2. Pedestrianisation will benefit many businesses who can continue to trade outside their buildings demise through the use of pavement licenses, allowing visitors an opportunity for outdoor dining.
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<p>GDPR (General Data Protection Regulation) / Privacy Impact Assessment</p>	<p>Not applicable.</p>
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	Risk Description & Risk Owner	Original Score (RYG)	How We Manage It	Current Score (RYG)
A				
B				
C				
D				
E				

	<p>Background documents Any previous reports or decisions linked to this item</p>
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	<p>Relevant web links</p> <p>Any links for background information which may be useful to understand the context of the report</p>
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Public Realm Strategy

October 2021

Austin-Smith:Lord





Conduit Street looking north to the Cathedral

Contents

04	Introduction
05	Background
06	Analysis of Existing Public Realm
14	SWOT Analysis
18	Public Realm Objectives
19	Hierarchy of Street Uses
20	Wayfinding & Movement
25	Hierarchy of Pedestrian Routes
26	Principles of Public Realm Design
30	Streets and Spaces
46	Signage
48	Lighting
50	Public Art
52	Green Infrastructure

Figures

60	Analysis Diagrams
68	Figure 5. Public Realm Projects
70	Figure 6. Suggested Order of Project Priority

Appendix

72	Figure 7. Public Realm Materials
74	Cathedral Quarter Public Realm Materials
77	Market Quarter Public Realm Materials
85	Southern Gateway Quarter Public Realm Materials
91	Public Realm Projects Order of cost

* Additional Appendices

A Consultation Results -
Survey Responses

- Action Plan, provided by Gleeds

*Please refer to additional appendices as separate document
from this report.

Introduction

"Places affect us all – they are where we live, work and spend our leisure time. Well-designed places influence the quality of our experience as we spend time in them and move around them. We enjoy them, as occupants or users but also as passers-by and visitors. They can lift our spirits by making us feel at home, giving us a buzz of excitement or creating a sense of delight. They have been shown to affect our health and well-being, our feelings of safety, security, inclusion and belonging, and our sense of community cohesion." - National Design Guide, Planning practice guidance for beautiful, enduring and successful places. Ministry of Housing, Communities & Local Government, 2021

"Have nothing in your streets and urban spaces which you do not know to be useful or believe to be beautiful" - William Morris

Although William Morris was originally referring to the 'house' and not our public spaces, between the two quotations above is encapsulated all we are trying to achieve within this Public Realm Strategy for Lichfield City Centre.

The Public Realm Strategy springs from the Lichfield City Centre Masterplan, which was developed by David Lock Associates in 2020 and provides a blueprint for the development of the city over the next 20 years. The Masterplan identifies a number of projects and proposals including new developments and public realm improvements, which are designed to complement and connect one with another.

These proposals have been developed from six identified Masterplan objectives; which are:

- A Strong Historic Core
- Complementary & Supporting Uses
- Welcoming Gateways
- Vibrant Streets & Spaces
- Quality Accessible Environment
- The "Green" & Sustainable City

Of particular relevance to this study, the Masterplan identifies the need for an "attractive and usable public realm", as one of four design principles aimed at ensuring design quality across all the objectives; this principle is the glue to bind the various strands of the Masterplan together. From this underpinning principle can be derived five further objectives that help set the template for this study.

These are:

- Removal of street clutter and promotion of design simplicity using a coordinated approach to signage and wayfinding across the city centre. A key component of this is that any public realm works must be robust, low maintenance and of a design suitable to a historic city centre.
- Allow for pedestrian desire lines in the design and layout of development. This is especially important for pedestrian cross routes around the city centre linking together key parts of the city, giving consideration of Staffordshire County Council's Local Cycling and Walking Infrastructure Plan (LCWIP)
- Consideration of landscaping opportunities that positively impact upon biodiversity within the city centre, paying particular attention to the Council's Local Plan 2015 evidence base (Nature Recovery Network) which discusses the habitat creation opportunities within urban cores.
- A coordinated but limited palette of surface materials
- Coordinated specification of street furniture components.

The purpose of the Public Realm Strategy is to set out a basis for achieving these objectives in the public realm. It does so by providing a cohesive approach to the unifying elements of urban design such as street design, signage, lighting, public art, green infrastructure and materials.

Also of importance is the Council's Local Plan Allocations (2019), from which a series of policies covering planning, design, transportation, accessibility, biodiversity and other issues have now been adopted for development management purposes. Until the adoption of Local Development Framework Documents, which will supersede these policies, they provide the context along with the SPD's within which development proposals within the city must be progressed.

The Lichfield District Local Plan Strategy 2015 identifies the priorities and objectives for Lichfield, which are set within a broader vision that by 2029, "... residents of the District will continue to be proud of their community, experiencing a strong sense of local identity, of safety and of belonging. Everyone will take pride in the District's history, its culture, its well cared for built and natural environment, its commitment to addressing issues of climate change, and the range of facilities that it offers. Our residents will have opportunities to keep fit and healthy, and will not be socially isolated."

So, with a keen eye on the broader vision within the Local Plan, this study is a key device to move the approved City Centre Masterplan forward and help guide the public realm aspects of development over the next 20 year period.

Background

"A good city is like a good party – people stay longer than really necessary because they are enjoying themselves." - Jan Gehl, urbanist

Lichfield is an historic cathedral city with a significant number of heritage assets (including several Grade I Listed Buildings) located within the Lichfield city centre Conservation Area. The historic character of the city centre is a key attraction to visitors and residents alike, and Lichfield is well-placed for the new trend of its experience-led offer to attract visitors to the city. The city has a number of leisure and cultural venues and an extensive festival, concerts and events programme to cater for its tourism economy.

With the rapid shift in consumer demand, the success of city centres is no longer measured simply by the quantum of retail footage alone. How the city centre is used and understanding what are the attractors are, is a far more informative and relevant method of analysis. So following this new thinking, a study of the pattern of usage within Lichfield, in terms of pedestrian footfall, was recently undertaken by Springboard and concluded that Lichfield has a Speciality signature.

What this means is that the pattern of footfall is a consequence of the key characteristics of speciality towns and cities which comprise:

- An anchor which is not retail, in Lichfield's case - Heritage
- Attracting visitors but serving the local population
- Having longer dwell time
- Focus is on protecting identity and positioning
- Offering something unique and special

These characteristics are immensely valuable, and Lichfield is the only city in its sub-region that is defined as a Speciality town/city. These attributes and characteristics must, therefore, be protected and the potential monopolised. Interestingly, one of the key characteristics of Speciality towns and cities is that footfall is as high or higher in the peak summer months than in December.

This affords Lichfield an opportunity to differentiate itself from other towns and cities, and to deliver a unique offer that capitalises on its Speciality signature type.

However, despite its compact nature, parts of the city centre can feel disjointed due to poor legibility and signage. The city centre has a variety of public spaces that have recently benefited from investment, including Beacon Park and Minster Pool. However the development of the public realm across the city has occurred piecemeal,

without a holistic vision and approach, or the guidance of a consistent set of design principles. As a result, the streets and links between these areas are often confused and poorly signposted, sometimes unattractive and hinder an ease of intuitive navigation around the city centre.

Analysis of the Existing Public Realm

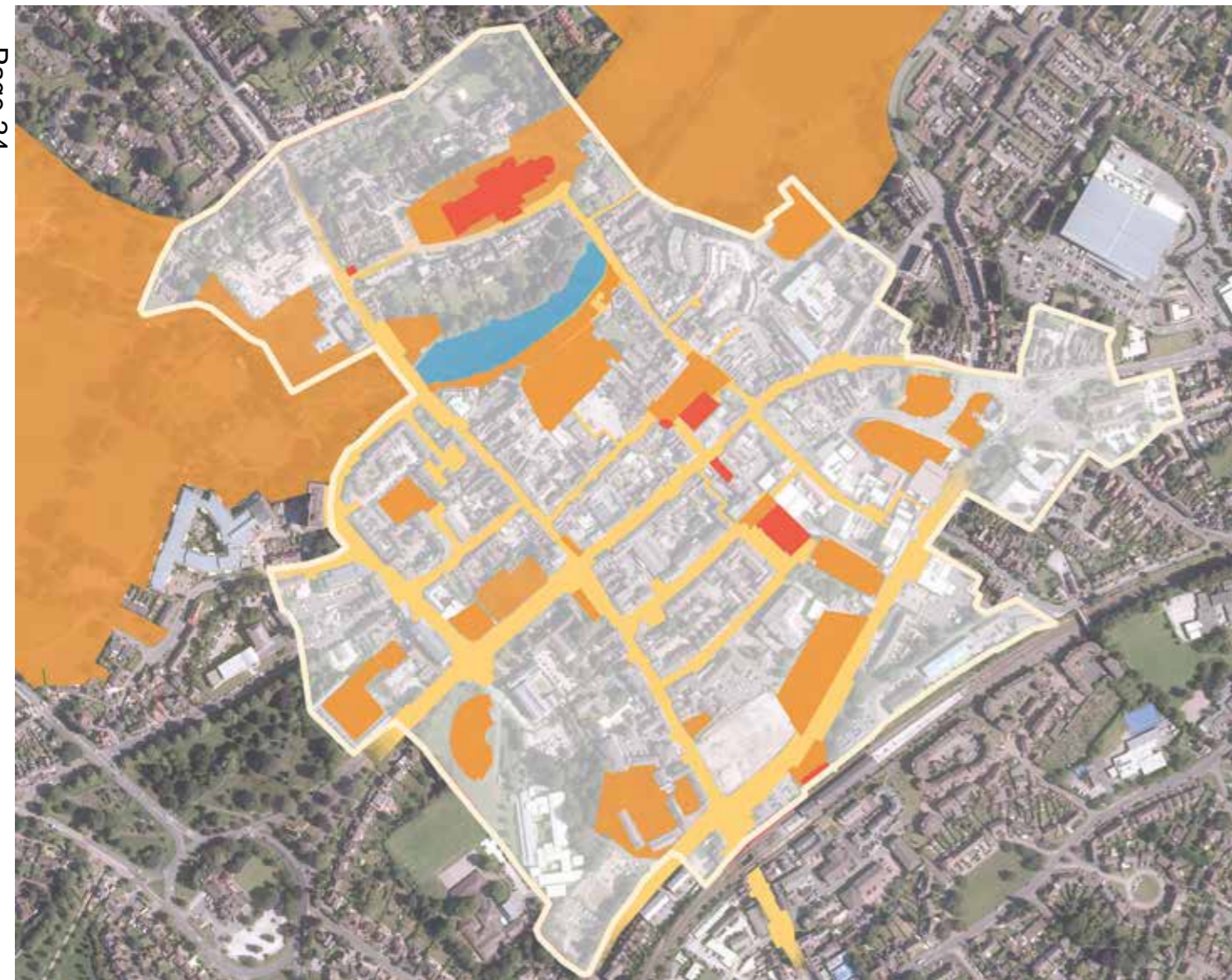
Lichfield's Public Realm

Lichfield is blessed with a rich legacy of heritage buildings set within a compact, structured, attractively scaled network of historic streets and spaces. As such, this is not placemaking, but a placemarking exercise, the true purpose of which is to enable the buildings and spaces to shine and provide a stage for the public use, enjoyment and appreciation of their city. When the structure and built elements of the city are performing well, the public realm does not need to shout and can play an elegant supporting role.

The sequence of urban spaces and the streets and alleys that connect them and form the public realm network is shown on the diagram below. What is noticeable is the relatively sparsity of public place within the historic core. The majority of the places indicated are the parks and open spaces of Beacon Park and Stowe Pool to the west and east and the car parks and transport hubs to the south.

Lichfield's Public Realm Key

- Landmark** Buildings
- Places** and spaces across the city centre
- Network** of streets and alleys that connect them



Page 24

Placemarking Elements

The quality, consistency and condition of street surfaces, signs, bollards, cycle racks, bins and seating have a significant influence on the perceived quality of the public realm. It has a key role in unifying streets and spaces and bringing identity to the city centre.

There follows an overview of the components of the existing public realm, with a more detailed analysis of the various materials, elements and components, their condition and location, included in the Appendix at the back of this report- please refer to Summary tables.

Streets and Alleys

Street Materials

The strong character and structure inherent in Lichfield's historic street pattern is often undermined by inappropriate, inconsistent and badly maintained materials. A number of surface treatments are present in Lichfield's city centre area, including concrete block paving, concrete slabs, brick paviors, stone paving and coloured tarmac. The apparently piecemeal application of surface materials has happened over a period of time and complicates the streetscape, doing little to complement the setting of historic buildings and aid the orientation of pedestrians.

The streets and spaces around St Mary's Church and along Tamworth Street and Bore Street have been recently renewed with quality, predominantly natural surface materials and present a palette of finishes that complements the various building forms, colours and textures - stone, brick and render.

Street Furniture

Many streets and spaces in Lichfield present a clutter of uncoordinated street furniture and signage that often obstructs pedestrian movement and hides much of the town's character. This is unfortunate and unnecessary, as ironically, much of the historic street furniture adds to the distinctiveness of place and visual interest within the public realm.

As with surface materials, the furniture in Lichfield is very inconsistent. There is a significant variation in colour, shading, size and form. This is apparent with the various types and styles of bollards, which are heavily used in Lichfield to delineate streets and spaces and protect pavement areas.

Fingerposts, bollards and other items such as cycle racks, and benches are also often poorly sited and in a poor state of repair, further undermining the streetscape and the quality of pedestrian experience. Through their location, signposts frequently impede, as opposed to facilitate, pedestrian movement.

The plethora of signage, with different styles of sign often fulfilling the same function, is also confusing and should be brought together in a coordinated manner.



Some elements of signage provide an attractive and interesting addition to the street scene, for example, the Heart of England Way pavement markers.

Similarly, certain elements of street furniture are carefully crafted with a combination of complementary materials that complement the architecture and scale of the street. These, perhaps, point the way forward.



Railing detail: Cathedral Close (above)

Spaces and Places

City places are critical to the urban life of the city; it is here that the visitor and resident gather, meet and basically participate in civic activity. The squares, promenades, parks and gardens are breaks in the network of streets where the space is available to slow the pace and step aside from the urban flow.

Their design and layout is frequently determined by the intended use and the character and response to the space can then be categorised according to this role:

Arrival spaces - points of arrival for the visitor to the city - the rail station, bus station and car parks. Often busy points of interchange or connection where people meet or orientate themselves before setting out on their trip. As a first impression, these are important urban spaces for the city, that frequently set the tone for the visit. Clear, unambiguous information is essential. Currently these are the least successful of the city spaces, where vehicular utility over-rides the human and pedestrian experience.

With the exception of the Bird Street Car Park these spaces are peripheral to the core city centre, where they are best placed to pick up the visitor and enable modal change.

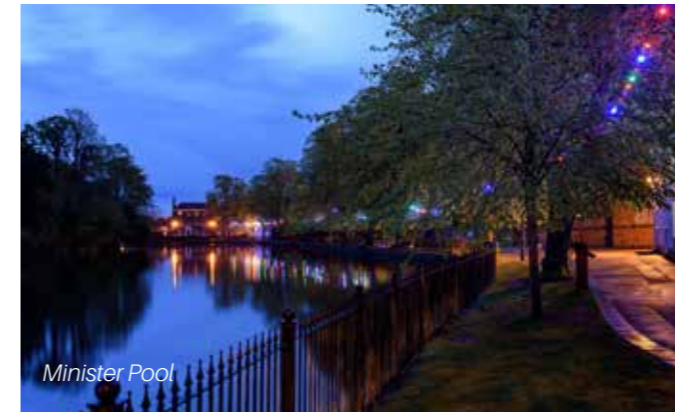
Active Spaces - closely connected to the primary pedestrian movement network, these places are lively and animated by activity; a stage for public life - eating, drinking, markets, people-watching, meeting, festivals and events. Market Square would be such a space. Given the variety of uses that might occur, flexibility and adaptability is important.

Relaxed Spaces - frequently the parks and gardens, but with gentler through movement of cyclist and pedestrians. Interaction is still sought and invited, but also the ability to draw away into quieter, more sheltered areas.

Tranquil Spaces - Often away or concealed from the primary movement routes, areas for quieter reflection and conversation. Remembrance Gardens would be a good example.



Page 25



Lighting

The lighting of the core city centre area is from building mounted fittings, which removes the clutter and obstruction of light columns within the public realm and the detrimental effect that highways lighting can have on the scale of historic streets. The lighting levels are generally low intensity and muted, with the wall mounted fittings washing light onto the façades of the buildings and creating a pleasing effect of silhouetting activity within the street.

The light fittings themselves are generally low pressure sodium, which although giving a warm light, has poor colour rendition, that loses the various colours on the façades and paints them all with an orange light. There is also a perception of insecurity after dark associated with these fittings as facial recognition is made more difficult.

A number of commercial properties have adopted their own lighting schemes for their premises, often using brighter and/or coloured lighting effects. Whilst this can bring animation to the street façade, if uncoordinated can detract from the holistic qualities and character of the streets and spaces, and place an unwarranted emphasis on particular buildings to the detriment of the composition and true hierarchy of civic buildings.

A balance also needs to be achieved with shop lighting, where certain overly bright shop windows can again disturb the balance within the street, dazzling the onlooker and spilling light onto the pavement.

Beyond the historic core, street lighting is generally to highways standard, yet there is space and scale within the road corridor to accommodate such light fixtures and fittings. However, the contrast from the more muted, wall-mounted fittings and highway lighting is marked, and an intermediate form and standard of lighting would be appropriate to bring the scale down to that of the pedestrian.

Maintenance and Management

A problem with using a broad palette of paving materials and styles of street furniture is the logistics of maintenance. Where works to the paving is required, multiple types of materials need to be kept in stock or arrangements put in place with numerous suppliers. This often results in surfaces being patched with concrete or tarmacadam, regardless of the original design material. This applies equally to street furniture, which often goes unrepaired or maintained, lending an air of neglect to the street and fostering further abuse or damage.



Highways & Traffic

Detrimental to the current city public realm and a challenge of the ambitions of the masterplan, is the increasing volume and speed of vehicular traffic, generating barriers to active travel movements, reinforcing separation between areas and undermining the qualities and scale of the historic city. This is particularly marked to the Birmingham Road Corridor, separating the rail station from the city, St John's Street leading into the Friary, and Swan Road leading northward into Bird Street. It is hoped and planned that the completion of the Southern Bypass will remove traffic, particularly HGVs, from Birmingham Road, further opening up opportunities for an enhanced urban environment, reinforced through legislative restrictions of traffic on the road.

Within the city centre core, restrictions are already in place to reduce traffic volumes and types of user to certain times of the day and days of the week. These restricted areas are shown on Figure 5 in the Appendix. The restrictions are confusing with both pedestrians and drivers unsure of where and when certain vehicles can enter these key central areas. A separate study is underway on Pedestrianised Streets and sets out to clarify this situation. The assumption for this study is that restrictions will be more stringent, extensive and enforced across the core city centre area.

Development Sites

Two key development sites were identified within the Masterplan, Birmingham Road at the heart of the Southern Gateway Quarter and Bird Street Car Park. These two sites will be brought forward in the short to medium term and their development provides not only additional facilities and amenities, but presents real opportunity to address current placemaking issues, create new public spaces and repair the fabric of the city centre public realm. This strategy, therefore, provides specific guidance on the incorporation of public realm within these two developments.

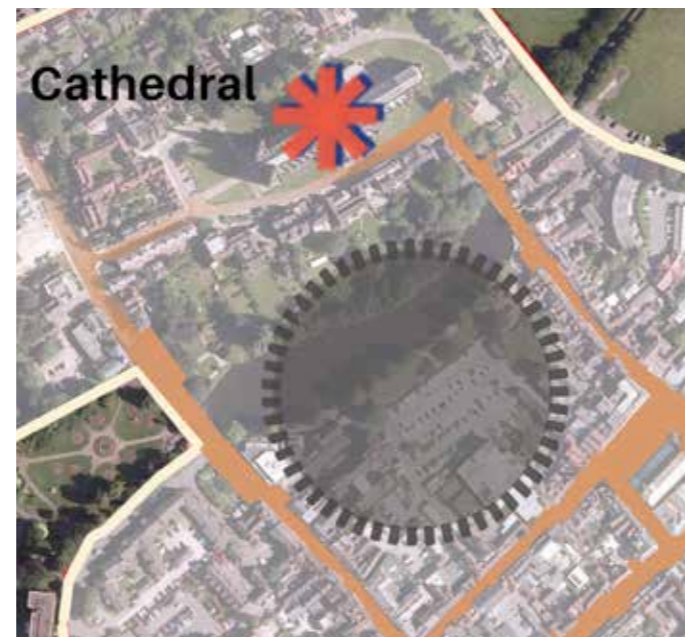
A further two development sites at University West Car Park and District Council House, will conform to the guidance as applicable to the quarter, but are not specifically referenced within this report.

Birmingham road Development Site

The current hoarded site presents an unattractive frontage at a primary gateway to the city centre with other associated areas of development currently maintained in a 'meanwhile' state.

Bird Street Car Park

The development of the Bird Street Car Park would address a significant structural and placemaking issue of the hole this area creates in the public realm of the city centre.



Bird Street Car Park - the hole in the public realm (above)

The prospect from this space is off the backs and yards of retail properties and a high brick wall to the north. Pedestrian and cyclist access from the west is via the busy car park access and from Market Street to the south,

via the narrow and uninviting Bird Street Walk. The car park itself is a busy and extensive expanse of cars and traffic that discourages any pedestrian movement across the space and is completely at odds with the qualities of public spaces within the rest of the city centre.

A further repercussion of this void in the public realm, is the perceived separation of the cathedral from the city core and the fact that the visitor is steered toward the rear of the cathedral from St Mary's Church along Dam Street.



Cathedral to town, a sense of disconnection (above)

Gateways and Entrances

A51/St Johns Street

The A51 approaching from the south and the M6 Toll is historic entry point of the London Road. Today, the gateway into the city is marked by the railway overbridge and the junction with the equally busy Birmingham Road.

Crossing the Birmingham Road junction, St John Street to the Friary is a busy, noisy, traffic dominated corridor with narrow pavements, but contains some elegant and important listed buildings including the Grade I listed Hospital of St John Baptist without the Barrs and its adjacent chapel, which sits to the west of the Birmingham Road Junction with St Johns Street and has marked the gateway to the city since the early 12th century. The Grade II listed buildings and attractive gardens that are part of the Lichfield District Council Offices are located to the east of the street, with a route through from St Johns Street to the car park to the rear of the offices.

The Friary

The Friary is the main vehicular route in the city centre from the west and The Friary Car Park is well located directly off The Friary to accommodate the visitor to the city centre. However, whilst the car park is only 240 metres from the junction of Bore Street and St John's Street, approximately a three minute walk, it is perceived as being distant from the city centre and is underused. There is a need to improve the connectivity between car park and the city centre, through improvements to the pedestrian connections along The Friary and via Sandford Street.

Lloyd's Walk

The area of Lombard Car Park borders the major open space of Stowe Pool, and the car park also provides parking for the cathedral, with access along Cross Keys and Reeve Lane. However, the most direct connection to the city centre from the car park is through the somewhat fractured urban structure around Cross Keys and then the tight and (particularly after dark) claustrophobic alley of Lloyd's Walk.

Questions on perceptions of personal safety within the public consultation exercise reinforced the impression of issues around Lloyd's Walk as a means of access and gateway to the city centre with many people expressing concern particularly after dark and on winter's evenings.

Beacon Street/Bird Street

The gateway into the city centre from the north-west, falls downhill past the former Angel Croft hotel site to the west and the Close, the point of entry to the Cathedral Quarter, to the east. In crossing the listed Bird Street Bridge, the route then transitions through the green corridor of Beacon Park moving into Minster Pool, before entering the secondary gateway of the Swan Road/Bird Street junction. North of the bridge and the Pool, the pavement widens out to enter the Remembrance Garden, before narrowing once more to cross the bridge.

To aid the diagonal pedestrian crossing of the junction of Swan Road, from Beacon Park to Minster Pool Walk, a traffic table has been created in the area of the junction, however, the crossing is constrained and awkward with poor sightlines and little space on the pavement landing zone to the southeast. Although this has the impression of being a dangerous junction, accident statistics would not support this, although it is heavily disliked and a concern to the pedestrian.

Church Street/Tamworth Street.

The A5127 is a busy road feeding into the city and bypassing immediately to the south along Birmingham Road. To enter the city from the west, the driver would

pull off the A5127, Church Street into Greenhill and Tamworth Street. The whole area of this extensive junction is dominated by traffic and highways infrastructure, with guard rails, crossings, traffic lights and signage set in a sea of tarmac. Once past the George Lane junction, on entering into the city, the setting improves as the scale reduces. Pavements here are very narrow however. On entering the area of traffic restriction, as the highways swings into Lombard Street, St Mary's Church dominates the view along Tamworth Street and the quality of the public realm improves with high quality natural materials and well proportioned delineation of the street.

Trees

Urban trees add great benefit to the communities that live around them. As well as being aesthetically pleasing, they have a positive effect on our environment, our health and well-being, our economy, and exist as a protection for the future.

Page 27

Trees in our streets and urban spaces help counter the impact of climate change, They cleanse the air, cool the ground, and hold back the pulse effects of heavy rainfall, thereby reducing flood risk.

They reduce stress, beautify the city scene and add value to a city's attractiveness and, therefore, economy.

A well-treed corridor extends from Beacon Park, across Minster Pool to connect to Stowe Pool in the east. Similarly, numerous street trees have been planted within the streets and frontages of the Learning Quarter. With the strong wooded backdrop of Station Road, the city centre is effectively ringed by tree planting. The historic centre of the city itself is, by contrast, hard and constrained for space, providing little opportunity for street trees. Where they are present they make an important feature within the streets and spaces. Notably at the junction between St Johns Street and The Friary, at the entrance to Bird Street Walk along Market Street and the mature tree within the small square north of The Garrick Theatre. These trees create a strong visual break to the architecture, providing a shady canopy in summer and form a magnet to which people congregate. New street trees have been planted within Market Square, which, as they mature, will greatly enhance the focal nature of the place.



Where trees are maturing within the pavement zone they are causing root heave due to compaction and insufficient growing space beneath the paving, lifting the surrounding pavement.



Paving effected by root heave (above)

Minster Pool

The trees to the rear gardens and northern edge of the Minster Pool form a backdrop to the pool and the foreground to the cathedral rising beyond. The softer edge of the northern bank contrasts strongly with the urban park edge to Minster Pool Walk to the south.

The wooded nature of the northern bank and restricted access creates an area of nature conservation value in the heart of the city. However, natural regeneration of elder and rhododendron lends an untidy and unkempt appearance.



Filtered, not screened views of the cathedral (above)

Whilst the yew trees in front of the Cathedral are intrinsic to the place there is a danger that, in conjunction with ivy growing up into tree canopies, they are overly screening, as distinct from filtering views of the cathedral. Over the centuries, numerous artists have painted the Cathedral from around Minster Pool and these views are in danger of being lost.

There is a balance to be achieved here and the trees must therefore be monitored to ensure that aspects and vistas to the Cathedral are not interrupted by overgrown vegetation. This is of particular importance given the views of the cathedral from Minister Pool Walk, Dam Street and Minister Pool Bridge are one of the finest experiences in the city.



Views to the Cathedral are interrupted by overgrown evergreen trees (above, left). Given many artists utilise this area for painting the Cathedral (above, right), vegetation monitoring and control is particularly pertinent.

OBJECTIVES	Strengths	Weaknesses	Opportunities	Threats
A Strong Historic Core	<ul style="list-style-type: none"> Easily legible, complete and contiguous core areas. Recognised through Conservation Area status and 'Speciality City' Status. Compact. 	<ul style="list-style-type: none"> Medieval 'ladder' street pattern restricts north/south movements to the edges. Little permeability through the core. Minster Pool should be a great asset for the city centre creating the foreground for the cathedral and strengthening connectivity between historic core and cathedral precinct, but currently is, if anything a buffer and barrier between the two (accepting that this was its original purpose!). 	<ul style="list-style-type: none"> Bird St Car Park development as a strong link between city and cathedral. Build on the existing views of St Mary's and the Cathedral. 	<ul style="list-style-type: none"> Traffic 'Highways' infrastructure
Complementary & Supporting Uses	<ul style="list-style-type: none"> Varied, niche existing retailers and food offer gives strong base to spring from. Speciality city status and cathedral provides destination vitality to the city centre 	<ul style="list-style-type: none"> 	<ul style="list-style-type: none"> Consolidation of various, but complementary uses (cultural, leisure, tourism, retail) to create a 'circuit of destinations'. Masterplan looking to promote more inner city living. 	<ul style="list-style-type: none"> Pressures and exigencies on high street retail. Loss of business confidence and increased vacancies due to Covid.
Welcoming Gateways	<ul style="list-style-type: none"> Clarity and compactness of the city centre area enables strong gateways to be identified and marked. Cathedral spires visible from most gateways. 	<ul style="list-style-type: none"> Excessive space required for traffic movements and conflict with pedestrians at the gateways. Levels of traffic Southern entry points particularly challenged and traffic-dominated from St Johns St/Birmingham Rd junction along the Birmingham Road corridor to Birmingham Rd/Tamworth St junction. Poor signage and wayfinding (particularly south to north from station to cathedral). Eastern edges of city core flaky and ill-defined, with Reeves Lane and Lloyds Walk eastern gateways from Car parks and open spaces uninviting and in latter case poor perception of safety. 	<ul style="list-style-type: none"> Birmingham Rd and transport hub improvements, enhance the currently poorest gateways to the city from the south, either by road, bus or train. The quality and clarity of the built form negates the need for easily dated, inappropriate and quickly tired-looking features to be erected to mark gateways. 	<ul style="list-style-type: none"> New development blocks visual connections to cathedral Area demands of transport hub on urban space mitigate against pedestrian and cyclist. Failure to secure development agreement on Southern Gateway site. COVID19 - implications for comfort items such as bench seating.
Vibrant Streets & Spaces	<ul style="list-style-type: none"> A quality, consistent built form, punctuated by striking landmark heritage buildings and set within clearly identified character areas. 	<ul style="list-style-type: none"> Mish-mash of street furniture in various materials, colours and styles and from various era. Differing approaches to the design and materiality of the floorscape within the streets and spaces, even 	<ul style="list-style-type: none"> Increased resident population, brings vitality, pride and 'ownership' of the streets and spaces. 	<ul style="list-style-type: none"> Failure to secure consensus on prioritising cyclist and pedestrian movement through restricting vehicular access to core areas. Failing to ensure that the public realm remains a space available to,

OBJECTIVES	Strengths	Weaknesses	Opportunities	Threats
	<ul style="list-style-type: none"> • Quality materials and finishes within certain key areas and streets - eg. Market Square. • Retail, leisure and destination activity brings a good level of footfall to the city core. • Strong existing programme of festivals and events, with organisations in place. 	<p>within the same character areas, prevents a coherency and consistency to the centre, creating a visually disjointed appearance.</p>		<p>and serving all groups within the community.</p> <ul style="list-style-type: none"> • Loss of local distinctiveness and sense of place.
<p>Quality Accessible Environment</p>	<ul style="list-style-type: none"> • Compactness of city centre. • Appreciation of the role and access requirements of a successful 'Speciality' city. 	<ul style="list-style-type: none"> • Confusing restrictions within pedestrianised and priority areas. • Traffic flows within TRO areas detract from streetscape and ease of pedestrian and cyclist movement. • Lack of consistency with signage design and location. 	<ul style="list-style-type: none"> • Improved public transport provision and transport hub, leading to modal shift away from the car. • Improve facilities and infrastructure for cyclists and pedestrians • Improved wayfinding for pedestrians and cyclists. • Modern, clean, flexible, real time, public transport systems through and around the city core. • New pedestrian crossing points at Birmingham Road corridor 	<ul style="list-style-type: none"> • Failure to secure consensus on prioritising cyclist and pedestrian movement through restricting vehicular access to core areas. • Lack of achieving the required organisational change to match any cultural shift as the city centre evolves. • Tightness of the city grid and ability to accommodate multiple modal movements - walking, cycling, vehicular, access to blue badge parking.
<p>The "Green" and Sustainable City</p>	<ul style="list-style-type: none"> • Strong network of linked open green spaces to north - Beacon Park, Minster Pool, Stowe Pool, bring nature into the city. • Trees and landscaping contribute to the character of Museum Gardens & Minster Pool and Birmingham Road. • Shaded areas to footpaths and cycle routes from some areas of the existing built and natural city environment. 	<ul style="list-style-type: none"> • Restricted widths of historic streets across the city and the density of the built form create challenges in increasing tree cover and implementing SuDs. • Little tree cover to the south of the city centre. • Limited number of recycling bins 	<ul style="list-style-type: none"> • New developments sites to the Southern Gateway and Bird Street Car Park provide opportunity to enhance tree planting and SuDs. • Reducing the space available to the car could free space for green initiatives within the street corridor. • Manage existing trees and plant new and replacement trees to mitigate losses in the last decade. • Improvements for wildlife? • Enhancement of green infrastructure is multifunctional in both tackling climate/ecological emergencies and serving the needs of the community. • Measurement of embodied carbon for infrastructure - future data capture for LDC to improve future projects. • Energy generation for signage and external lighting etc. 	<ul style="list-style-type: none"> • Reorganisation of, and demands on the street corridors fails to generate space for green infrastructure. • Increased maintenance requirements including potential water use and management. • Sustainability of the supply chain for materials and street furniture etc and limited ability to alter this within the remit of the Strategy. • Requirements for utilities diversions - cost implications

Public Realm Objectives

The National Design Guide is structured around ten characteristics of well-designed spaces and places that work together to create its physical Character, nurture and sustain and sense of Community and work to positively address environmental issues affecting Climate.

The ten characteristics are:

- **Context** – enhances the surroundings.
- **Identity** – attractive and distinctive.
- **Built form** – a coherent pattern of development.
- **Movement** – accessible and easy to move around.
- **Nature** – enhanced and optimised.
- **Public spaces** – safe, social and inclusive.
- **Uses** – mixed and integrated.
- **Homes and buildings** – functional, healthy and sustainable.
- **Resources** – efficient and resilient.
- **Lifespan** – made to last.

These ten characteristics are captured in the following diagram:

Page 30



With the exception of 'homes and buildings', all of the characteristics would apply to the public realm, so extending these characteristics to Lichfield, the public realm will be designed to:

- Meet the needs of Lichfield as an attractive, distinct and thriving economic, tourist and social centre in the region

- Achieve high quality spaces using durable materials incorporated into uncomplicated designs focusing on the correct balance and proportions of elements within the street
- Encourage innovative design, avoiding pastiche by reinterpreting the historic fabric through the use of contemporary, timeless design
- Avoid street clutter and co-ordinate furniture, signs, posts, bus shelters and lighting to respect surrounding buildings and the overall street scene composition, whilst bringing clarity and continuity to aid wayfinding
- Provide a unifying structure, drawing together the historic and more contemporary elements of the city into a legible whole
- Improve connectivity and integration between areas, celebrating their individual histories as well as realising Lichfield's potential as a 21st Century city
- Promote sustainable transport modes and particularly encourage pedestrians.
- Provide a just and equitable access to areas of public realm which meet the needs of all groups in society
- Enrich the evening economy and offer a safe and rewarding visit
- Be carried out in phases if budgets are limited, rather than compromising quality
- Meet the needs of maintenance requirements and recognise replacement and whole life costs of materials
- Utilise locally sourced natural materials and skills where possible

A more fine-grade level of guidance and of immediate relevance to the heritage-rich city of Lichfield can be found in Historic England 2018, 'Streets for All', and this has been a constant point of reference in drawing together this design strategy.

The successful design of the public realm will be achieved through the involvement of multi-disciplinary design teams including artists, landscape architects, highway engineers, urban designers, lighting designers, as well as consultation with the public, local businesses and user groups.

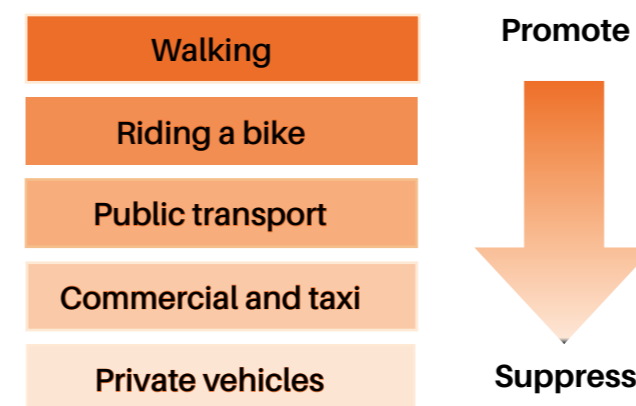
Hierarchy of Street Uses

The compact structure of Lichfield, in common with many medieval settlements, results from their historic function as regional centres for trade and artisanal craft, with the necessary squares, marketplaces and short walking distances. This is also an ideal structure for an urban movement pattern centred on walking and cycling, and encouraging social interaction, footfall and dwell time.

The focus (or return to a focus) on the human dimension in planning our city spaces, placing the needs of the pedestrian and cyclist above those of the private car, would go a long way to meeting the key objectives of the Lichfield District Local Plan Strategy 2015. That is, to create a city that is healthy, safe, prosperous, lively and sustainable.

Developing this principle, Local Plan Strategic policy 2 (SP2): Sustainable transport, includes making provision for "widening travel choices and making travel by sustainable means more attractive than the private car" and the Local Plan document explains that "active travel is part of sustainable transport, it seeks to improve people's physical and mental health by using urban design principles to give pedestrians, cyclists, and users of other transport that involve physical activity the highest priority when developing or maintaining streets and roads. This can mean reallocating road space to support walking and cycling, restricting motor vehicle access, introducing traffic-calming schemes, and creating safe routes to schools and childcare settings."

Effectively the policy creates a hierarchy of travel modes, prioritising space within the public realm to promoting green, active means of travel - walking, wheeling and cycling. This hierarchy is illustrated in the diagram below. However, it must be applied sensibly to recognise the needs of the broader transport and highway network and it is increasingly important to consider the movement of services and goods, not just people. Nevertheless, this hierarchy is a clear statement of the Council's priorities for movement in the city, and this must be reflected in the design of all streets and spaces.



Wayfinding & Movement

The Pattern of Streets

The orthogonal grid of the medieval ladder street pattern, in conjunction with the visibility of the spires of the cathedral and St Mary's Church, enables an ease of mental mapping and wayfinding, particularly from the key gateway of the rail station.



Views of the landmark spires from the rail station (above)

For the visitor arriving in the city at the station, the visibility of St Mary's and the cathedral also enables a ready appreciation of the compact scale and easy distances involved. The immediate impression is that walking and cycling are appropriate and, therefore, encouraged.

However, the reality of moving through the city centre is heavily challenged by the impenetrable nature of development along the cross streets (the ladder rungs of Wade Street, Bore Street and Market Street). This tends to move north-south movements to the perimeter of the historic core along St John Street/Bird Street and Baker's Lane/Conduit Street/Dam Street - the two ladder rails.



the medieval ladder street pattern (above)

A new central route

Our proposal, therefore, is to create a new central route from the station to the cathedral that connects many of the landmark buildings, places and spaces of interest within the city centre. This is perhaps a more intuitive route through the city, which would supplement without replacing the current movement patterns. It would also lock the Birmingham Road development site into the spaces and street network of the city.

This broader proposal would also draw the visitor to the front of the cathedral taking in the well established vista from Bird Street Bridge.



A new central route through the city (above)

Fundamental to the success of this proposal is to advance the ideas from the Masterplan to drive a new connection from Market Street to Minster Pool and develop an extended pocket park to the north of the existing Bird Street car park, creating a city frontage space onto Minster Pool.

The new route would not only address the uninviting nature of Bird Street Walk. But would open up views of the cathedral from Market Street, aiding orientation and the alignment of the route would keep the cathedral central to the view as the pocket park and Minster Pool are revealed.

Minster Pool Circular Walk?

In the only significant change from the Masterplan, we would propose not to adopt the concept of the circular walk around Minster Pool, as this would impinge on the Remembrance Garden, would cross private land and impact on the ecology to the north of the pool. Rather, we would wish to promote and enhance the existing circular route from Minster Pool Walk up to and through the Cathedral Precinct via Bird Street and Dam Street. Stakeholder and public consultation would suggest that this decision would be well-supported.



The revised Minster Pool circular walk (above)

Gateways & Entrances

Making gateways of our car parks

There are three multi-storey car parks in Lichfield that border the city centre to west, east and south and to help reduce traffic movements within the city centre we have to make our edge of centre car parks attractive and easy to use. These car parks are only a few minutes walk away from the historic core of the city and must become the gateway for the car user.



Multistorey carparks - gateways to the city centre (above)

A separate study is looking at how the car parks can themselves be improved, but it is the aim of this study to explore how once the car is parked, the journey to the city centre for the pedestrian is clear, safe and attractive.

As discussed later in the chapter, this starts with providing good information online to the visitor before they embark on their journey, but on arriving to the city, the car parks need to be clearly signposted. These car parks are well located for three of the four main routes into the city,

The Friary Car Park

The Friary car park is 240 metres from the junction of Bore Street and St John Street, approximately a three minute walk. However, it is perceived as being distant from the city centre. We need to improve the connectivity between car park and the city centre, through improvements to the pedestrian connections along The Friary

A important secondary connection to the heart of the restaurant offer on Bird Street exists from the north-eastern exit of the car park to Swan Road and along Sandford Street.



Connections to The Friary car park (above)

Lombard Car Park

The area of Lombard Car Park borders the major open space of Stowe Pool, but the most direct connection to the city centre is through the somewhat fractured urban structure around Cross Keys and then the tight and (particularly after dark) claustrophobic alley of Lloyd's Walk.

Whilst there is little significant improvement to the Cross Keys corridor that can be achieved through public realm interventions alone, there are moves that can be made to improve Lloyd's Walk.



Connections to Lombard Car Park (above)

Birmingham Road Gateway Car Park

Accessed directly from Birmingham Road, adjacent to the Three Spires shopping centre and located within the Birmingham Road Gateway site, this car park is ideally placed for shoppers and visitor to the city centre arriving from the south. If a new car park is to be developed on this site it must allow pedestrians easy access into the city centre.

The Rail Station

It is important that the Birmingham Road Gateway development's linkages with Lichfield City Station becomes the start point for pedestrian and cycling routes around the city to promote active travel and provide a hub for the active travel network. A cycle hub could be developed within Station Square, providing secure cycle storage, information, bike hire and repair.

As discussed below, the visitor should be able to orientate themselves and pick up all the information they might require for an enjoyable and successful stay in the city.

The design of the Birmingham Road Gateway should ensure that views of the cathedral and St Mary's church are retained and framed from the new station square to aid immediate orientation.

Wayfinding Information Systems

The user experience will be dramatically enhanced by taking a whole journey, coordinated approach to online information, services and physical elements. The user, whether they be a visitor or resident, will gain a greater understanding of the city and have a more enjoyable experience when provided with consistent, high quality information at all stages of the journey.

Presenting the city centre as a singular composition reinforces the sense of unity and continuity through the streets and spaces. Grasping and carrying an impression of the full expanse of the city centre as you move around, reinforces the mental map - connecting destinations and aiding orientation.

Lichfield Visitor Information

Develop and extend the existing Visit Lichfield website This website is frequently a visitor's 'first point of contact' experience of the city, and must capture the principles of the whole journey and total composition as discussed above, and reflect changes to the public realm, routes and new developments.

This digital gateway should be tailored to user requirements and create an enhanced experience in terms of guiding, journey planning, attraction and destination finding. The website will capture the visitor before they

- embark on their journey to the city and will:
- Introduce the unique visual identity of the city to the visitor at the start of their journey experience - as they plan their journey online.
 - Provide a geographic overview of the city to reveal Lichfield's unique setting, walkable scale and main visitor attractions.
 - Extend the overview to present an understanding of the development of the city through the ages.
 - Provide visitor itinerary planning tools - to reveal the city's offer and encourage longer stays.
 - Consider an interactive mapping suite to create a personalised themed maps and guides of the city.

Walk map Review and amend the free printed visitor map for pedestrians which can be distributed through main points of arrival, the new transport interchange, tourist information centres, attractions, destinations, accommodation providers, universities and other third parties.

Content to include:

- Visitor information.
- Attractions and destinations.
- Interpretation.
- Cycle and pedestrian routes.
- Transport connections.
- Content indices.

Themed maps/guides Enhance and extend the range of themed maps and guides to connect attractions, destinations and points of interest. To be provided in multilingual versions, themes could include:

- heritage and culture,
- shopping,
- Lichfield after dark,
- city centre walks and parks and gardens.

Available in printed format from main points of arrival, transport interchanges, tourist information centres, attractions, destinations, accommodation providers and other third parties.

Digital information services In conjunction with the signage strategy, develop a digital information strategy for the provision of information services to static digital information points and dynamic hand held mobile devices. Develop contextualised mapping information for interactive online and on-street digital use. Review and audit technology to adopt a future proof approach to providing, managing and maintaining visitor information that can be accessed by hand held mobile technology.

Arrival points Extend and coordinate information signs at key points of arrival including Lichfield City Railway Station and primary car parks, to provide welcoming visitor information and point of orientation for navigation and onward journey planning. Information to include:

- Welcome to Historic Lichfield.
- An overview or prospect map to allow users to view the full extent of the city. Users will then be able to understand its distance, structure and physical relationship of destinations. In a simplified form, this composition will be a consistent element that remains with you as you move around the city.
- Instructions on how to get around the city.
- Onward journey planning information.
- Primary visitor information including primary attractions and destinations and interpretation, pedestrian routes and transport connections.
- Content indices.

Pedestrian signs Develop pedestrian information signs to be located at key decision making points in the city centre. Information will include:

- A street level of mapping, for their next destination/ point of interest, that will enable people to navigate their next step through the city and find nearest destinations or facilities.
- A map of the extended city centre for orientation within the wider context of the city.
- Primary visitor information including primary attraction and destinations and interpretation, pedestrian routes and transport connections.
- Content indices.

Shopping directories Develop a range of information directories to provide visitors with detailed information about their immediate area.

- Shopping directories to allow shoppers to find specific destinations quickly or to identify the full extent of retail options available.
- Food and drink directories.
- Information directories at key destinations and attractions.

Interpretation points Develop a range of interpretation signage such as historical plaques, interpretation signs, city vista displays and public art interpretation, to provide information about Lichfield to enhance a visitors experience through engaging, revealing, understanding, discovering and learning.

Interpretation products will include:







- Detailed guides, indexes or interpretation will be provided annotating an elevation of the view. Information will help users explore the area in greater detail.
- Historical interpretation.
- Reveal further points of interest in Lichfield.
- Restore incised lettering of street names.
- Links to further information.

Hierarchy of Pedestrian Routes

The plan below illustrates an assessment of the routes and spaces of Lichfield and describes a pedestrian route hierarchy, centred on the new proposed medial route through the city. Visitor Information Points are also shown.

This process is not static and relies on continued assessment of current and future development sites in the city to ensure these are connected into the network. The primary pedestrian routes and secondary connecting routes link arrival points and destinations in the city, guiding the location and orientation of the four basic types of information elements. The indicative location of these elements form a core network which will, in time, be extended into new development areas such as the Birmingham Road Site.

The hierarchy of movement and signage Key (below)

	Primary routes		Arrival points
	Secondary routes		Interpretation points
	Tertiary routes		Pedestrian signs



The hierarchy of movement and signage (above)

Hard landscape elements: paving and street treatment

A number of character areas within the city centre have been identified within the Masterplan. All too often, recognising a diversity of character is met by one of two responses: a standard approach to every part of the public realm; or a 'bespoke' approach to each of many sub-areas. The former can mask the character it should be celebrating; the latter leads to an uncoordinated patchwork of elements that often date very quickly. Finding the appropriate ground between these extremes is one of the purpose of this Public Realm Strategy. However, it would not be our intention to apply a different range of materials and elements within each of the city quarters and we strongly lean towards the consistency of a common palette of materials and fixtures, applied sympathetically to the context.

Wall to wall paving solutions affect the proportions of the street and diminish visual interest within the ground plane. This may be appropriate within a setting of contemporary buildings but does not sit as comfortably within an historic street pattern. For this reason, a pavement zone will be identified within the historic streets, regardless of the presence of upstand kerbs, or the kerb height. This approach is supported by Historic England in their guidance document 'Streets for All', where they call for maintaining kerb-lines to preserve the historical form of streets.

Where the carriageway is used for unrestricted vehicular access, it is important to keep a kerb height of at least 60mm and to use different materials to clearly define the separation.

A further advantage of this principle is that within pedestrianised areas, the 'pavement' zone can also delineate the area of shop or café spill out onto the street, maintaining the 'road' zone for pedestrian movement. Again, a principle supported by Historic England.

For both contemporary and historic street façades, as well as providing shade, shop awnings can enliven the street scene and greatly assist in humanising the scale of the street by capturing ground beneath the awning for the pedestrian to stop and linger. It is also a useful device to delineate spill out space for cafés and restaurants.

Surface Materials

We will commit to the use of robust, natural paving and materials across the public realm, applied with a view to whole life costs, which encourages the reuse of locally sourced materials wherever possible. This use of indigenous materials applies a contextual grain to the streets and spaces, which should be continued to the detail of their application. There are a number of existing street

details in kerbs, channels, cross overs and back of paving infill that lend a continuity, scale and distinctiveness to the public realm and should be extended or reinterpreted across the city centre area.

General Guidance

Workmanship

The key to successful paving is threefold, quality robust materials, appropriate detailing and a high standard of workmanship. The standard of setting out and laying of the paving is key to a long-lasting quality finish. An experienced laying team would be constrained by overly detailed layouts and what they require is a series of principles that the mason can follow on site.

Consistent application is important for maintaining a clean aesthetic. Footway surfaces should be firm, slip resistant, low in reflectivity, laid in a manner which is comfortable underfoot, and minimises the risk of trip hazards and is well drained.



Construction

Structural design depends on the level of everyday use, the risk of vehicle overrun and the existing ground conditions. The relevant standard local guidance must be used to design the pavement.

In trafficked areas, where vehicles are prone to mount

the pavement it is not necessarily appropriate to install lines of bollards which will only add to the street clutter. Paving slabs laid on flexible base courses will inevitably be damaged by overriding vehicles. Where vehicle overrun is likely, paving slabs should be laid on a concrete base and tapped down to ensure continuous support with no air pockets.

Laying patterns

Small-module paving on footways is best avoided unless there is a historical precedent. It tends to dominate the street, especially where traditional footways and kerb lines have been removed. It may sometimes be better to use concrete flags or asphalt rather than fragment the floorscape. Small modules are best confined to the carriageway and pedestrian crossing points, and may not be suitable for areas highly trafficked by HGVs, particularly in areas of braking and turning.

Paving should always be laid perpendicular to the line of the kerb in staggered rows.

When specifying construction material choices it is important to give consideration to likely needs for repair and maintenance, including sourcing replacement materials. This is obviously simplified by limiting the palette of materials.

Joints and Cuts

Cuts to slabs should be used to achieve changes in gradients. Generally the number of cuts should be kept to a minimum. Residual slab lengths of less than 150mm should be avoided. Where two footways intersect at awkward geometries, paving should be cut to ensure a clean aesthetic and respond appropriately to the road hierarchy and the building line. A preferred approach is to cut the paving units in a radial, fan pattern. Generally this approach should be reserved for wide or long corner radii and as a response to the adjacent building architecture, and where a maintenance strategy has been agreed. A cap stone may be required at the inner radius and double units can be used on tight radii towards the inner radius.

Mortar

Where it is used, mortar has a significant impact on the aesthetic quality and overall appearance of the paving surface. Good edge restraint on both sides is essential to prevent spreading. Where footways do not abut a kerb or existing wall, an edging is required. Clean joints at kerb edge and back of footway are required by careful detailing and cutting pre-construction.

Mortar infills must be avoided at the backs of kerbs, at building façades and around utility covers. Infills should be kept to a minimum around posts and special core drilled

flags are recommended to ensure good fitting.

Dropped Kerbs

The number of diagonal cuts required to attain the level change around the dropped kerb should be minimised and should not leave small segments. Vehicular overrun in these locations should be anticipated and the construction and detailing should be carefully undertaken to prevent the paving quickly failing.

Alternatively, quadrants can be used to return the kerb into the footway forming a single gradient between levels avoiding any diagonal cuts.

Special Kerb Types

The use of special kerbs should be considered in certain circumstances to improve appearance, assist installation and ultimately, facilitate movement within the street.

Considerations would include:

- Use standard quadrants at all 90 degree external junctions.
- Typically, ramped kerbs, are only required to ease the transition of wheels (prams, wheelchairs, trolleys etc.) over the up-stand.
- High containment profiled kerbs should be avoided in places with a high 'place' value.
- High access kerbs should be installed where practicable at bus stops.

Cycling Delineators / Demarcation

Up-stand kerbs separating cycle lanes and tracks from pedestrian areas should generally be avoided unless they are the result of retaining an existing carriageway kerb.

A standard pre-formed delineator (photo) should be used and if vertical separation is absolutely necessary then a low (maximum 60mm) splay kerb should be used.

Tactile Paving

To be effective, tactile and hazard warning paving materials must be applied in the correct form and in the correct place, to comply with the requirements of their intended function. However, they must also be sympathetic to the adjacent surfaces and character of the surrounding area. The city centre is a designated conservation area and therefore a tactile surface should be employed that blends rather than contrasts with the surrounding materials.

Brass studs with non-slip surfaces provide a contrast in colour and texture without being visually intrusive. Corduroy and tactile paving can be supplied in natural stone to suit the adjacent material.

Cutting

On site cutting of kerbs should be avoided where possible.

Where necessary, all cuts should be neat and tidy, fitting adjacent outlines.

Quality control of paving

A sample panel should be constructed at the start of the construction phase to establish specified standards of workmanship for the scheme, acting as a quality benchmark. Typically a sample panel area would cover around 30 square metres of footway and represent most features in the build, including a kerb edge, building line, inspection covers, a radius and at least one dropped kerb.

Street furniture elements

There is so much in our streets that shouldn't be there in the first place; or that has a theoretical function that it is not fulfilling; or which is fulfilling a useful function but could be better-placed. For the first of these groups, remedial action requires little more than the allocation of modest resources to clean, tidy, remove or enforce. For the second - such as the removal of guard-rail that are serving no practical safety purpose - the justification for removal will need to be properly investigated and documented. For the third group, signs and other useful street kit can often be moved out of the main walking desire line or combined on one post or column, rather than two or three.

The city currently contains a myriad of different products, and whilst each may do its specific task adequately, there is little consideration to their collective impact.



To realise a range of elements that sympathetically responds to the city's character and design aesthetic, it is recommended that the certain general principles are followed:

- The street furniture selection should be a coordinated contemporary range implemented throughout the city centre. Street furniture should not reflect any heritage style or imitate a point in history;
- The materials must be durable, able to resist vandalism and be easily maintained. Ease of replacement must be considered;
- The designs should be simple, stylish and elegant capable of accompanying a range of settings;
- The placing of the elements must respect pedestrian flows allowing unimpeded access along the footways. Equal consideration should be given to the proposed function and the needs of disabled and less able users;
- The design and the materials should be contemporary, but reflecting a timeless quality.



To assist with improving legibility and engendering a sense of place, subtle modular variations to the standard items could be introduced.

As with the signage, naturally coloured materials should be employed for robustness and authenticity, however, coloured detailing could be incorporated in the form of steel end brackets, infill strips or illumination to reflect the quarter in which the furniture is situated.

Possible colours to complement the tones and hues of Lichfield could include:

	Pale green RAL 6021
	Cement grey RAL 7033
	Signal yellow RAL 1003
	Pigeon blue RAL 5014
	Anthracite grey RAL 7016

Pop-up power and water points

Pop-up power and water points would be located within areas of the street and within public spaces that are suitable to house events, markets and festivals. The use of these facilities would remove or reduce the need for dirty and noisy generators and water bowsers and pumps. Water points would also facilitate cleaning during and after events.

EV Charging Points

There are currently a few EV charging points around the city and located within The Friary car park. These need to become more prevalent as the number of electric vehicles increases on our streets and to encourage the uptake of this clean technology. The design and location of these elements requires the same level of consideration as all other aspects of the public realm, to avoid obstruction and clutter.

The Furniture Zone

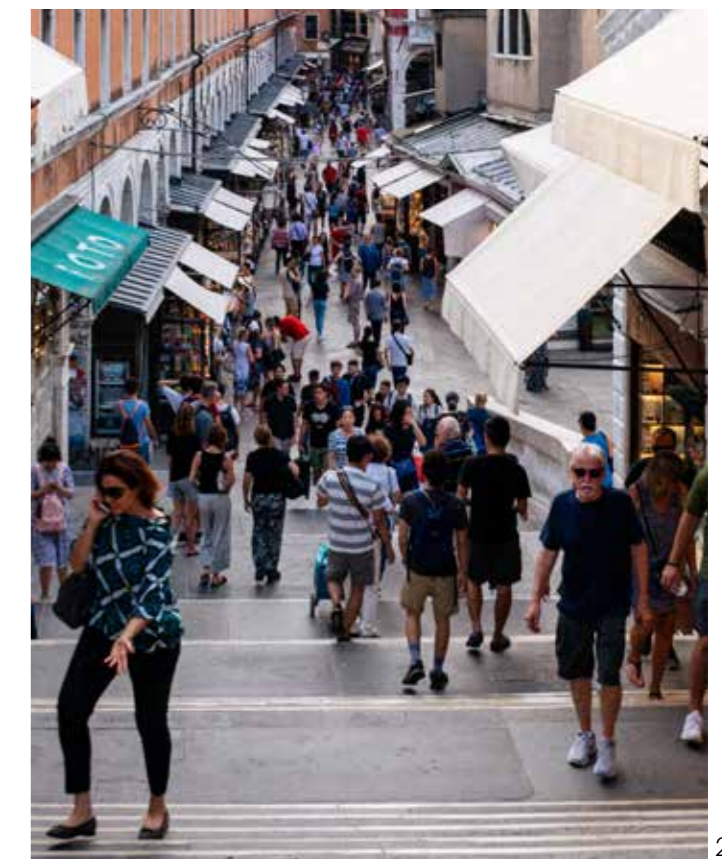
The footway width is created to allow for effective width for pedestrians, and as such footways must be kept clear of any clutter. Any necessary items of furniture within a footway should be organised within a furniture zone at kerbside (where a kerb exists), to prioritise space for pedestrian movement and allow for improved inclusivity and accessibility.

Shop Frontages

The quality and character of the shop frontages across the city contribute immensely to the overall quality of the public realm. The use of colour and typeface, the scale and proportion of glazing and signage, retaining or revealing historic facade elements and continuity of building lines contribute significantly to the overall consistency of the street scene.

Awnings can enliven the street facade and capture space in front of shops and businesses, reinforcing the movement and use patterns, suggesting a place to pause or linger; but they need to be sympathetic to the general appearance, scale and treatment of the public realm.

Specific guidance on Shop Fronts and Advertisements is provided in Lichfield District Council's Historic Environment Supplementary Planning Document (page 29 - 32), but should be read in the wider context of the guidance within the document.



Introduction

The development of concepts defining the materiality and appearance of the street scene within the various quarters of the city centre is derived from the function, qualities and character of the area and of the particular street. Whilst we are advocating a consistency of materials across the entire city centre, with subtle variations to finish, pattern and unit size, these can be applied uniquely and distinctively in response to their immediate context.

So, unsurprisingly, and with a few minor deviations as identified below, the approach to the design of the public realm across the city centre responds closely to the delineation of the four Quarters identified in the Masterplan.

Public Realm Projects

The following section of the report outlines the approach to the treatment of the streets and spaces that make up the public realm of Lichfield city centre. Individual projects, defined by the street or open space, are illustrated in figure 6 within the Appendices and numbered as below.

Cathedral Quarter
 The ultimate responsibility for the public realm of the Cathedral Close is outside that of the Local Authority and so the following is primarily observation and recommendations that could be incorporated either through collaboration between the parties or independently by the Cathedral.

The materiality of the Cathedral Quarter is already of a high quality, contextual and complementary to the palette of materials proposed for the wider city centre. Little change is therefore envisaged to paving, kerbs and channels, although there is a need to review materials in certain areas to more robust finishes to withstand the pressures of vehicular overrun and heavy foot traffic.



Cut stone drainage channel within pavement - Cathedral Close

It is important to retain and replicate authentic details that provide character to the street. The cut yorkstone dished channels that run from downpipe to kerb channel across the footway would be an example.

Similarly items of furniture are frequently of heritage value, well designed and proportioned and should be retained. However, certain elements of street furniture need addressing to remove clutter and simplify the scene and here it is hoped this would continue the city wide approach; for example, signage, bollards and lighting.

Cathedral Quarter Materials

Footways	Yorkstone paving (riven) 500-700mm coursing x random length
Kerbs	Yorkstone 180mm wide x 125 high x random length
Channels	River cobbles set in mortar with black granite cube edging.
Carriageway	Fibredec or similar
Shared Surface	Yorkstone sett, random coursed.

Market Quarter

As described earlier, the streets within this quarter centre on, and radiate from, the urban block containing St Mary's Church, The Corn Exchange and the Market Square. This central hub, therefore sets the precedent for street surfacing materials within the quarter (and across the city centre) and the recent works to the public spaces around St Mary's has established a palette of high quality, predominantly natural materials, not only appropriate to the historic core areas, but also, in modified finish and format, to new development sites.

For this study, we are working on the assumption that traffic restrictions will be more stringent, extensive and enforced that is currently the case across the core city centre area.

Market Quarter Materials

Footways	Yorkstone paving (flame finished) 500-700mm coursing x random length
Kerbs	Yorkstone (sawn) 250mm wide or black/pink granite 140mm wide x various heights x random length
Channels	Dark grey/black granite 250mm wide x random length
Carriageway	Random coursed tumbled concrete block, multi
Shared Surface	Yorkstone sett, random coursed.

Projects:

1. Market Square/Breadmarket Street/Bore Street/Conduit Street/Tamworth Street

With the exception of the stretch of Bore Street between Breadmarket Street and Tamworth Street (Bore Street East), the existing, recently laid paving to these streets conforms to the overarching principles, quality standards and the route hierarchy, and would be retained. Certain areas of damaged paving would be repaired.



Natural stone paving and tegula carriageway on Bore Street

As with all the other projects outlined, however, to achieve a consistency, clarity and continuity across the public realm, the cross-city schemes for lighting, wayfinding, signage and street furniture would be implemented within these streets and spaces, replacing the existing provision.

2. Bore Street (East)

This short stretch of Bore Street between Breadmarket Street and Tamworth Street, is nevertheless an important part of the city centre public realm, forming the southern side of the historic central block, with Conduit Street and Baker's Lane (Three Spires) also feeding in to it. New paving conforming to the proposed palette surrounds this section of road, highlighting the condition of the monotonous and tired, concrete block paving that currently exists.

The proposal would be to bring this section up to the same standard as the surrounding public realm, through new paving, extending the Market Quarter paving palette. Paving would be kept flush across the width of the street, but channels and a change in material and module size from the pavement to the carriageway, would delineate the streetline. The line of the existing drainage channel would be retained and enhanced.



Concrete block paving on Bore Street (east)

The paving extends beneath the under-croft of the shops to the south of the road, and the new paving would extend into this area, with careful cutting around the pillars.



The under-croft area on Bore Street (east)



Bore Street East- dashed line indicating the threshold where paving changes from stone to concrete.

3. Market Street

A key retail street, Market Street is lined by shops and cafés, a number of which spill out onto the street. The existing monotonous concrete blocks would be replaced with the proposed palette. A new delineation of the street form would be established, with channels and a differentiation in paving module between footpath and carriageway, defining the area for tables and chairs. Kerbs would be flush, and the line of the channel would be strongly defined to accentuate the line and visual continuity of the street.

The existing trees and seating area would be retained but enhanced with new materials and fittings. Cycle parking would also be provided in this area. With the development of the Bird Street car park site, and a new Bird Street Walk connection onto Market Street, this would create a prime place to meet, rest, re-orientate and just watch the world go by.



Concrete block paving on Market Street



Eastern end of Market Street, with existing Bird Street Walk

4. Bird Street Gateway

The diagonal cycle and pedestrian route crossing from Beacon Park into the city centre occurs where traffic is turning northward from Swan Road into Bird Street, but also southward onto Bird Street to access the Bird Street car park.

This is also the location where Bird Street crosses the relatively narrow listed bridge at the head of Minster Pool, so the footpath to the east of Bird Street is narrow, with opportunities to widen curtailed by the bridge structure.



The narrow pavement and awkward crossing at the Bird Street/Swan Road junction

To the east of the bridge is the proposed alignment of the primary pedestrian and wayfinding route between the historic core area, Minster Pool and the cathedral precinct. It is important, therefore to maximise available space for the increased pedestrian flows by widening the footpath into the road. At the same time we are proposing to reduce the traffic speed and create a larger traffic table from west of the junction and up Bird Street to past the turning into The Close and the cathedral precinct.

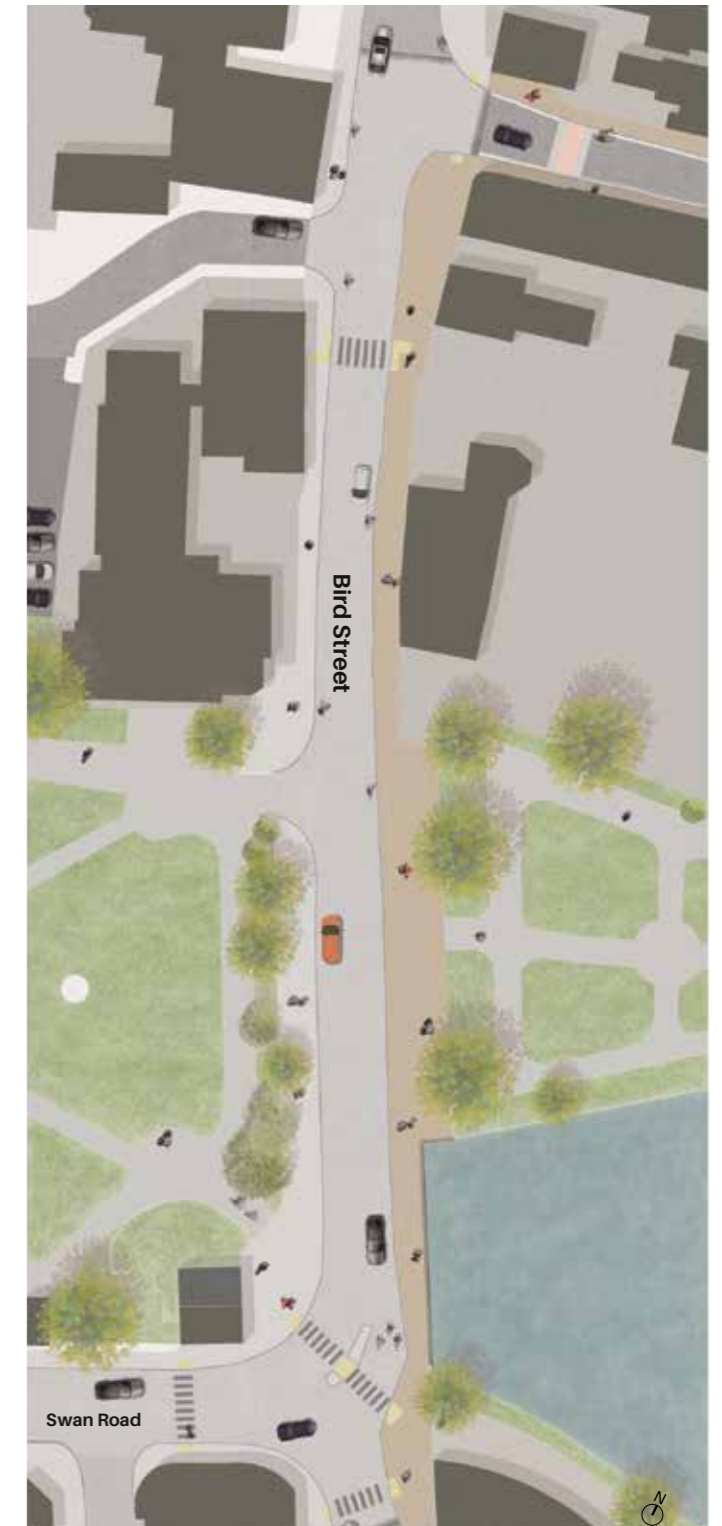
The traffic table would be in multi block paving to indicate to the motorist that he is entering and driving through a heavily pedestrianised area and reinforce the proposed



Paving and carriageway in natural stone and concrete multi block

speed restriction of 20 mph.

The diagonal crossing of the traffic table from the park would be strongly delineated with a adequately dimensioned refuge between the two traffic lanes.



Bird Street - Swan Road to The Close

5. Bird Street/Sandford Street/Car Park Entrance

Bird Street - Bird Street contains the primary food offer in Lichfield, lined with cafés and restaurants, frequently spilling tables and chairs out onto the street. To support the principle of delineating the pavement zones, the area into which outdoor eating extends, is contained by the channel that marks the carriageway. Defining the line of the road is also useful for safety, as although restricted to certain vehicular uses, traffic is still fairly regular on the street and a clear definition of 'highway' enables both driver and pedestrian to understand how to avoid each other.

However, the existing engineering brick paving used in Bird Street is failing, with channels units being replaced by concrete, areas of damaged paving being repaired with tarmac, and significant occurrences of the paving pumping under traffic loading, causing the integrity of the sand bed and jointing to fail.



Red and blue engineering brick used on Bird Street

The strong colours of the red and blue engineering bricks enable a clear differentiation between the carriageway and pavements along the street, but the colours are somewhat strident and the contrast marked, detracting from and occasionally clashing with the colours and textures of the architecture. Adoption of the proposed materials with flush kerbs and channels would present a more robust and visually sympathetic streetscape, but retaining the zoning of the existing materials.

Sandford Street - The proposal is to improve the signage, paving and road crossings along the route, but particularly to extend the pedestrian treatment of Sandford Street past the Horse and Jockey and up to the road junction at Charter House. This should include pursuing the replacement of the lighting columns with wall-mounted fittings and removing or replacing the heavy, aging timber bollards.



View east along Sandford Street



Illustration of Bird Street/Sandford Street existing paving

6. Dam Street

A secondary retail street, with a greater occurrence of cafés and restaurants than the retail core. Dam Street is part of the eastern rail of the ladder street pattern and directly connects the Market Square with the Cathedral Quarter. The street would be treated in a similar manner to Market Street with quality materials defining a pavement and carriageway zone centred on a strong channel line. Being a pedestrianised street, kerbs again would be flush.



Northward along Dam Street to Cathedral

The cross city pedestrian and cycle route flowing through Beacon Park and along Minster Pool Walk, crosses Dam Street to the east of Minster Pool and this junction is marked by Speakers Corner. This is an important, sunny, open pedestrian intersection, where people naturally gather and with great views of the cathedral over the Pool.

From here, the route continues either northward into Cathedral Close, or eastward along Reeve Lane to Stowe Pool. Cycle parking and seating would, therefore, be enhanced in this area.

Feeding off Dam Street to the east is the transition zone of Cross Keys, which is connected to the core area by Reeve Lane to the North and the narrow alley of Lloyd's Walk feeding into the square to the south.



Speakers Corner, Dam Street

7. Lloyd's Walk

To improve the experience of entering the historic core of the city and overcome the perceptions of safety inherent in this narrow alley that feeds into Market Square from Cross Keys and Lombard Street car park, it is necessary to address the causes and issues.



The narrow and uncomfortable Lloyd's Walk alley

A combination of artwork and lighting strung between the buildings could humanise and present a brighter, safer route.



Art and lighting working together

The blank brick wall would be transformed by the inclusion of artwork and softened by clothing in a green wall. The utilitarian galvanised palisade security fencing should be replaced with a more appropriate barrier. Importantly, discussions should be held with the landowners to see if it would be possible to drop the height of the brick wall that borders the car park to maintain clear sight lines along the alley and remove the area concealed from view from the entrance of the alley when approached from the car park to the east.

Bird Street Car Park

As described in chapter 5, for reasons of wayfinding and to open up a new central route through the city centre, the Masterplan proposal to redevelop the Bird Street Car Park and the B&M store on Market Street, would be supported. This move would address the key structural issue of the void at the heart of the city and create a brand new civic space, whilst also removing the narrow and intimidating Bird Street Walk.



This heart-space, fronting onto Minster Pool with the iconic backdrop of the cathedral, would become the Living Room of the city - where the community congregates to celebrate, relax and enjoy.

Any new build elements within the Bird Street Car park development should include green roofs and in open areas in the scheme tree planting should be maximised. The new route from Market Street through the car park site would lead to an enlarged and improved Minster Pool Gardens and would provide glimpsed views of the cathedral as a reference point, but retain the intimacy and enclosure of the historic core street pattern. Active frontages would provide animation and with lighting and clear sightlines would enhance the sense of personal security.

8. Minster Pool Walk and Gardens

Minster Pool and Gardens is a central feature of the city. A further proposal would be to float a permanent stage on the northern bank of the pool, on axis with the new approach path and central to Minster Pool Gardens. The stage would be accessed with necessary permissions from Dam Street and would be for occasional use during festivals and celebrations.



Impression of Minster Pool and Gardens as frontage to the potential Bird St car park Masterplan development.



A floating stage on Minster Pool for events and as an eyecatcher

9. Reeve Lane

The intention is to respond to and extend the green corridor or 'green lung' that runs west/east through the city from Beacon Park, through Minster Pool and onto Stowe Pool. Reeve Lane continues and connects to the national cycleway and footpath that runs to the north of Stowe Pool, and to Cross Keys that borders Stowe Pool to the west and runs southward to the Lombard Street Car Park.



Reeve Lane and the National Cycle Route that runs to Stowe Pool

Mainly a walking and cycle route, new tree planting and materials would draw the countryside right into the city and extend the qualities of a country lane along Reeve Lane up to Dam Street.



Reeve Lane - extending the countryside into the city

Reeve Lane Materials

Footways	Yorkstone paving (riven) 600mm coursing x random length (laid to north only).
Kerbs	Flush Yorkstone 250mm wide x 125 high x random length
Channels	Dark grey/black granite 250mm wide x random length
Carriageway	Fibredec or similar
Shared Surface	Yorkstone sett, random coursed.

Southern Gateway Quarter

Southern Gateway Quarter Materials

Footways	Yorkstone paving (diamond sawn) 500mm coursing x random length
Kerbs	Yorkstone 180mm wide x 125 high x random length
Channels	Concrete
Carriageway	Bitmac/concrete block
Shared Surface	Tumbled concrete setts, multi, random coursed.

Three Spires

In a similar context to the Cathedral Close, Three Spires Shopping Centre is not within Local Authority ownership and therefore, the control and responsibility for the design of the streets and spaces.



Three Spires - the historic Baker's Lane

However, although fronted by contemporary retail architecture, the Three Spires Shopping precinct follows the historic line of Baker's Lane, which in turn formed part of the eastern 'rail' of the medieval ladder street pattern. Also, in terms of the relationship to the historic core of St Mary's Church and the Market, Baker's Lane is a spoke radiating from this central hub, similar to Bore Street, Dam Street, Market Street and Tamworth Street. The section of Baker's Lane between Bore Street and the arcade leading to the theatre is also a part of the proposed new central wayfinding route through the city.

The treatment of the street, including street furniture, should respect this historic and structural framework and preferably present a continuity of materials. The design and application could subtly differ with the street pattern of road, kerb and pavement less defined and approached in a simpler, more contemporary manner.

10. Wade Street/Castle Dyke/Frog Lane

Situated within the Southern Gateway Quarter and functioning as residential streets with defined highway and unrestricted vehicular use, whilst also providing access and servicing to existing commercial and business properties. These access functions could increase as the Southern Gateway Development comes on stream. The footway and shared surface materials should be consistent with the natural palette elsewhere with the quarter, but the road carriageway would be macadam tying into St John's Street. The area of public realm around the Garrick Theatre will become a critical stepping stone between the Southern



The predominantly residential Wade Street looking towards the Three Spires Shopping Centre

Gateway Development and the historic core. At the moment, the frontage is dominated by the road and traffic on Castle Dyke, which if calmed and the roadway narrowed and resurfaced, creates the opportunity for a shared surface, with activity spilling out from the theatre and café, and new trees reflecting and extending the existing open space to the north, which in turn, feeds into the arcade entrance to the Three Spires.



Castle Dyke - the forecourt to the Garrick Theatre

Southern Gateway

Similar to the Three Spires Shopping Centre, the Gateway development site will be contemporary, but sympathetic to the architecture of the historic city. The treatment of the public realm should follow this lead, so the same palette of materials would be used as within the Market Quarter, but applied in a more contemporary manner in terms of finish and laying pattern.

Wherever possible, new built elements of the development should include green roofs to mitigate the relative paucity of greenspace within the city core and suitable space should be found for urban tree planting within the streets and spaces.

To assist with wayfinding, the vistas towards St Mary's and the Cathedral spires from the station forecourt should be retained in the design and layout of the blocks and streets making up the new development.



View from station forecourt to St Mary's and the cathedral

There is also an opportunity to create a new link through the Council offices garden from Frog Lane to St Johns Streets, thereby opening up a new greenspace.



Potential new route through Council Offices Garden from St Johns Street to Frog Lane

11. Birmingham Road/St Johns St and Greenhill Junctions

The scale of development varies considerably along the road from the bus station and the Three Spires multi-storey car park facing residential properties. The width of the highway, dominance of the traffic and expanses of tarmac are also visually harsh and uncomfortable for the pedestrian and cyclist.



Vehicle and highway dominated views along Birmingham Road



Birmingham Road (west) centred on the Railway Station

The proposal is to create space for the cyclist and pedestrian, reduce the carriageway widths and slow the traffic, and develop corridors for avenue tree planting and raingardens. The intention would be to use the landscape of the road corridor to unify or mask the disparity of scale of development and eventually, as the Southern Gateway development comes to fruition, create a central spine for the Quarter.

The proposals would also enable immediate access to a cycle network around the city, enabling the development of a cycle hub at the interchange, and an easier pedestrian crossing from the rail station into the city. The Heart of England Way also crosses Birmingham Road. Improvements to the rail station forecourt could be a quick win for the strategy.

Careful attention should be paid to the corner of Birmingham Road turning into St Johns Street to ensure there is sufficient space and set back from the traffic to accommodate comfortable pedestrian and cycle movements, as this will remain an important secondary route into the city.

To the east, the areas of, and around the Greenhill/Rotten



Vehicle and road infrastructure dominate Greenhill

Row/Birmingham Road/Church Street junction are an extensive expanse of tarmac with a clutter of bollards, lights, signs and railings. Whilst the geometry of the junction may be required, opportunities should be pursued to:

- reduce and rationalise the amount of highways clutter around this junction and;
- explore measures to include green infrastructure to mitigate the starkness and humanise the scale.

12. Station Square

The redevelopment of the Southern Gateway and the Birmingham Road site, together with the promotion of active travel, will establish the rail station as an important gateway to the city. The current environment of the station does not reflect this status and the proposal is to create a new Station Square. This new city space would be pedestrian-friendly, with taxis, drop off and parking tamed and controlled. A new cycle hub would be located in the square with easy access to the cycle network around the city.

The new square would be the southern terminus of the central wayfinding route through the city and should connect smoothly with the adjacent development site and then onward into the city.

13. St Johns Street

To improve the environment of St John Street from Birmingham Road to the Bore Street junction, it is proposed to restrict vehicular access to buses and key users only.

The benefits would be:

- to reduce the heavy flows of traffic,
- to improve access for residents and businesses,
- to enhance the setting of, and access to the listed St John Hospital
- to improve the public realm for the pedestrian and cyclist, in this instance, particularly visitors arriving by bus or train.



The narrow pavements of St Johns Street looking south to Birmingham Road

The detail of this concept and how it would be enforced would be subject to further study and consultation, but there are two options emerging. Option A limits the restrictions to St John Street between Birmingham Road and Frog Lane and Option B extends the restricted zone into The Friary, as far as the Premier Inn roundabout.

At public consultation, both options received a fair degree of support, but there was also a number of cautionary voices expressing concern about the perceived local and city-wide implications of reducing access and through traffic. It is our view that the concept has merit and is in line with the overall thrust of the objectives, but concerns raised would need examining and addressing.



OPTION A - Traffic restrictions within St Johns Street only
In Blue - Alternative vehicular movement
In Orange- restricted area



OPTION B - Traffic restrictions extend to The Friary
In Blue - Alternative vehicular movement
In Orange- restricted area

14. Tamworth Street/Lombard St/George Ln (parts)

Greenhill leading into Tamworth Street is an historic route into the city centre from the east and the lower section of Tamworth Street in particular retains much of these heritage qualities and proportions. Further east, the enclosure of the street is lost to the south, where the street is bounded by car parking and the wide junction arrangement at Gresley Row.

The pavement zone along this section of road is often restricted and this is particularly noticeable either side of the George Lane junction. Initial highways assessment would suggest that the road carriageway could be safely narrowed, whilst retaining the on-street short term parking, allowing the pavements to be widened.

Materials would be in line with the Southern Gateway palette and as the route forms an important vehicular access to the Cross Keys area and Lombard Car Park, the carriageway would be bitmac, to differentiate from the pedestrianised zone to the west of Lombard Street.



Tamworth Street looking up the hill towards Greenhill

Business & Learning Quarter

The character of this area is entirely different from the tight-knit intimate, historic core. Buildings are frequently set back from the kerb line and pavement and there is a greater density of tree cover, lawns and planting. This is a leafy, more spacious environment, entirely in keeping with the function as a business and learning district, with a campus-like feel appropriate to the presence of South Staffordshire College and Staffordshire University. The aim would be to enhance this contrasting vibe, whilst seeking to improve linkages to the city centre. This should be done by enhancing the space for pedestrian and cycle movement through the creation of leafy, tree-lined boulevards to the main arterial routes leading towards the city centre. Narrowing the space available within the road corridor for the vehicle would slow traffic and change the perception of use towards active travel modes.

The material palette could change to high quality concrete products in combination with natural materials and stronger colours could be introduced on the ground plane.

Business & Learning Quarter Materials

Footways	Yorkstone paving (flame) 500-700mm coursing x random length
Infill paving/trims	Tumbled, silver grey concrete setts 150 x 150mm and tumbled concrete setts, red multi, random coursed.
Kerbs	Conservation kerb, silver grey
Channels	Conservation channel
Carriageway	Bitmac
Shared Surface	Tumbled concrete setts, multi, random coursed.

15. The Friary

The Friary car park is 240 metres from the junction of Bore Street and St John Street, approximately a three minute walk. However, it is perceived as being distant from the city centre. We need to improve the connectivity between car park and the city centre, through improvements to the pedestrian connections along The Friary and towards Sandford Street.

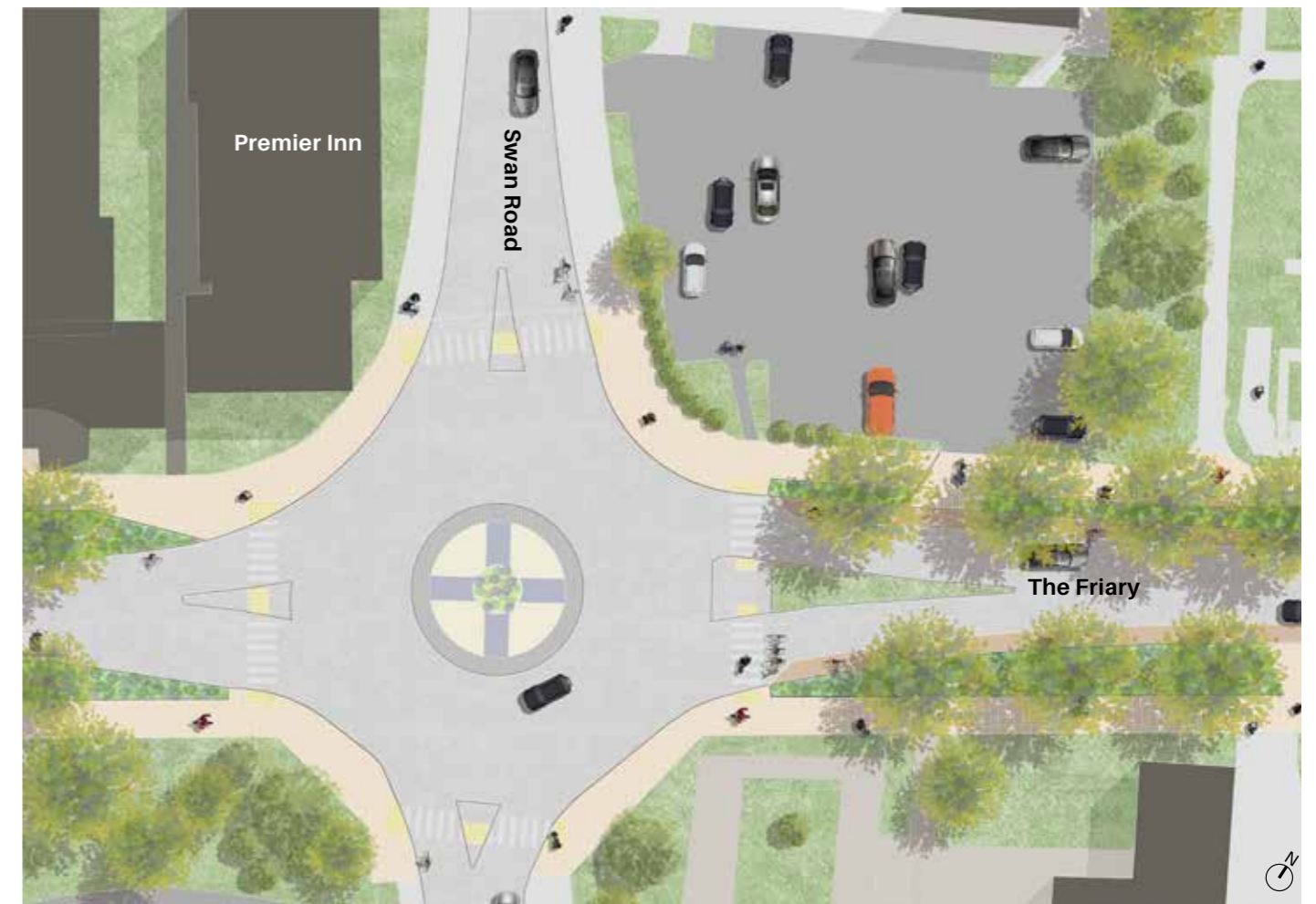
The Friary should become a boulevard with wider footpaths, provision of cycle lanes, and avenue trees and rain gardens to alleviate the effect of heavy rain events. All this can be achieved by reducing the road carriageway widths.

A continuity of paving materials, lighting and furniture and an improved pedestrian crossing over Swan Road would improve and perceptually shorten the journey.

An important secondary connection to heart of the restaurant offer on Bird Street exists from the north-eastern exit of the car park to Swan Road and along Sandford Street. The proposal would be to improve the signage, paving and road crossings along the route, but particularly to extend the pedestrian treatment of Sandford Street past the Horse and Jockey and up to the road junction at Charter House.



The Friary approaching Bore Street/St Johns St junction



The Friary junction and boulevard

Signage

Signage is a critical part of the Visitor Information System and this section should be read alongside - Hierarchy of Pedestrian Routes, on page 23. However, Signage also falls under other remits and responsibilities, for example health and safety and highways signage. This multi-agency involvement results in the uncoordinated appearance and clutter currently existing and previously described.

Signage design and location needs the same level of consideration as all other aspects of the public realm, so they become a positive element in the street and not detract from the overall image.

Principles to be adopted in the design and location are:

Clarity - all information displayed should be easily legible for the intended viewing position and viewer, but should only be present where the information is required and relevant.

A 'do minimum' approach - this starts with designing out the potential reason for a sign or road marking being required. For example, yellow lining is not necessarily required in Restricted Parking Zones; and railings and signs are not always required where pedestrian crossings are well-located on the pedestrian desire lines. Closely review necessary regulations and guidance to see how signage can be avoided, and if not avoided, then minimised.

A sequential approach - start with the minimum level of signage as above, and if more is seen to be essential, review closely and add cautiously.

Careful location - whilst ensuring legibility, position signs within the public realm to minimise visual and physical impact, enabling an uncluttered appearance and ease of movement. This could include attaching to existing buildings or structures (lighting columns) and certainly should involve planning multiple signs on the same column.

A coordinated approach - much investment within the public realm to achieve an attractive, uncluttered and pedestrian friendly environment, would be wasted or marred by the ill-considered spread of signage. Coordination is, therefore, required between departments and authorities responsible for the various types of signage, to work towards best practice and to take a holistic view of the cumulative impact on the city streets and spaces.

Timeless design - as with all the elements of street furniture, the design of the signage system should not follow any particular period style or attempt to conform to a general faux 'heritage' appearance. Rather the signage systems should be contemporary, contextual and applied

consistently. The signage system should look equally appropriate and at home within the Cathedral Quarter and the Southern Gateway Quarter. Any signs of genuine heritage value should be retained and if necessary, restored.

A Twin Approach

There is a risk in trying to display all the information required around the city on the same structures that the overall effect of the signage (no matter how well designed) becomes excessive and out of scale with the setting.

A twin approach is therefore proposed separating interpretative information from directional, shopping and visitor information.

Visitor Information System (VIS) - With respect to the Visitor Information System for the city, therefore, it is proposed to adopt a twin signage approach. Arrival points, shopping directories and pedestrian signs would all be included within a new signage system for Lichfield. These would be predominantly monoliths or information boards at key junctions and interchanges within and around the city, with finger posts either used in conjunction with, or as a directional adjunct to these.

The system should be designed as a set of components, flexible and easily adapted as requirements change.



Interpretation Points - Predominantly involved in the telling of the history of Lichfield, its streets, places, landscapes, buildings and people, these would take a more crafted approach to design, with the capacity to be monoliths, wall-mounted plaques or even installed in the ground. Elements of colour could be included to match the VIS Quarter colours. Not as visually apparent as the VIS, they would nevertheless form sculptural elements within the streetscape enriching the visual appearance.



Lighting

Page 45

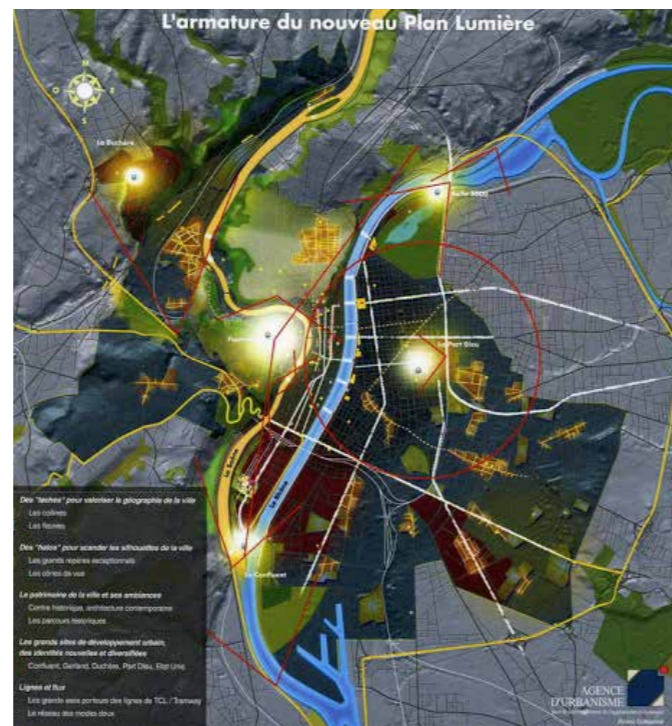


Building mounted light fittings on Tamworth Street

Given the framework of existing building mounted light fittings around the city centre, there is tremendous added value that can be relatively easily achieved through a coordinated lighting strategy. This would highlight key buildings and spaces contrasting with a background level of lighting in other areas and streets. The warmth of the light source itself can also be varied, revealing detail, creating visual interest and drama.

A key proposal is to make a holistic scheme of lighting for the city centre a priority public realm project. The transformative effect of a city-wide lighting strategy to the visitor and night-time economy is well tested and established. One of the early advocates was the city of Lyon in mid-France, whose Plan Lumiere and city beautification strategy fixed the position of Lyon as France's second city.

The initial proposal, therefore, is to brief and commission a Lighting Plan for Lichfield, that responds to and supports the approach to the streets, spaces and buildings within the Public Realm Strategy.



Lyon's Plan Lumiere

Principles for the Lighting Plan would include:

- Prepare a Lighting Plan for the city centre that considers a hierarchy of street lighting in conjunction with architectural feature lighting, including shop windows, floodlit buildings and feature trees.
- The lighting in Lichfield should be designed around human perception and not engineered for vehicles or to meet current lighting standards alone.
- Retain and restore historic lighting fixtures where possible, particularly in the historic centre and conservation areas. Incorporate modern, LED fittings.
- Where new lighting is introduced, the design should be in a simple, timeless, contemporary style that complements the street and other elements of street furniture. Pastiche lighting columns should be avoided.
- Use energy efficient fittings and luminaires and provide effective lighting controls to enable lighting levels to be varied to meet changing needs and moods.
- Lighting should be designed to limit light pollution. The use of full cut-off lighting may be appropriate in areas of architectural importance.
- In general, continue the programme of building mounted or ground fitted lights in preference to pole mounted lights in the city centre to minimise clutter.

- Lighting in areas where safety and security is an issue should conform to BS5489 3/2 Code of Practice for Crime and Safety.
- Regard should be given to Lichfield District Council's Environmental policies



Lichfield Guildhall



Different warmths of light at Halifax Piece Hall

Public Art

Experiencing Public art is playing an increasingly important role in the enjoyment, appreciation and understanding of our public realm, and becoming central to the culture of contemporary city life; but to achieve this, it must be carefully considered and implemented well. Too much, badly conceived or poorly executed and what should enrich can suddenly become clutter, a distraction or obstruction.

The public realm strategy establishes a coherent pattern to analyse and understand the city, expressed through Quarters, gateways, linkages and movement. Translating these concepts into tangible reality requires coordinated policies and cross departmental action. It is recommended that a strategic approach to the introduction of future public arts is developed to conform to this process. This will ensure that the location, scale, function and procurement of future art work complements and enhances the vision of the city.

More than this though is the concept of assisting in telling the story of the city, a story that is still unfolding and will ensure that Lichfield city centre is both appreciated by, and relevant to new generations.

Public lettering and signage should also be considered within the context of public art, with the intention of creating a unique and specific public realm. Standard products and letter faces should make way for individual design which reinforces the legibility and identity of the city and its quarters.



Liverpool Central Library - Literary Carpet

The Lichfield City Sculpture Trail provides a route around the city to explore much of the public art on show and describes the story behind each piece of artwork. Much of the sculpture visible in the city is associated with the cathedral, but there are however pieces of note to be seen around the city, both historic and contemporary.

Encouragingly this legacy of artwork is still being added to, with recent pieces from 2019 in the Erasmus Darwin House (Nautilus by Hannah Golding) and the more recently erected statue of St Chad by Peter Walker within the cathedral close.

The guide is available digitally and provides an accessible way of navigating the city by art. However, the trail guide is restricted to sculpture and does not provide a full picture of artwork available to the public; for example, the wonderful 'Christ in Majesty' stained glass window in the Chapel of St John the Baptist by John Piper and Patrick Reytiens, would not be included.

As part of the City of Sculpture programme, curated by Peter Walker, contemporary soundscapes and video are geo-located to specific sites around the city enhancing the visitor appreciation. These will be expanded over time.



Lichfield Cathedral Son et Lumiere 2016

In a similar vein, temporary art programmes in Lichfield have produced challenging and innovative work by artists with a global reputation which has attracted regional and national attention.

More recently temporary artworks included Lichfield Lumiere, in which the cathedral itself became the focus of a five day son et lumiere event. These temporary and recent art installations demonstrate that challenging work made by artists working in contemporary media can sit alongside the historical fabric without compromising the heritage asset or spoiling people's enjoyment of the historical fabric. In fact, temporary art projects have transformed, for a brief period, some of the most important and valued sites in the city and offer a way to challenge accepted thinking about the role and nature of art in a heritage context, while sidestepping the often convoluted approvals process associated with more permanent artwork.

Animation

Lichfield is known as the festival city and beyond pure public artworks, Lichfield's lively events and festivals also have a significant effect in animating the public streets and spaces of the city. Street theatre, music, funfairs, processions and guided tours create an attractive buzz in the streets. The city is increasingly attracting visitors who make repeat visits to enjoy the ever-changing programme of events, exhibitions and attractions.

Lichfield's aim, expressed through its artwork and events, would be to enhance its reputation as a city which combines its rich history with a forward-thinking, innovative approach to contemporary culture.



Lichfield Food Festival

Green Infrastructure

TREES

GENERAL CONSIDERATIONS

Trees and woodlands make a very positive contribution to sustainable urban living. They should be a key feature of almost all streetscapes and public spaces, regardless of the typology and character of the place, as their benefits are numerous and well documented.

Given the characteristics of the streetscape in Lichfield, it would be virtually impossible to plant 'too many' trees, as constraints placed on their satisfactory location will necessarily prevent this from happening. Projects, therefore, should seek to establish the maximum number of trees appropriate to the street or space's size, scale, character, functional requirements, constraints and design intent.

Large trees are generally preferable to small trees (their beneficial effects are generally amplified by size) but choice of tree type will obviously be influenced by the sites programmatic needs, constraints, soil conditions, micro climate, establishment and maintenance regimes.

In most instances to provide visual interest all year round, planting mixes should be dominated by native and/or naturalised deciduous species, as these best reflect the climatic climax vegetation of the Midlands. That said, opinion is starting to differ on this point with an increasing call for more exotic, non-native species to be planted in our urban areas, to offset the risk of wholesale denudation should infection strike our native species. The tree planting strategy, therefore, on any project should be developed with the tree officer in the first instance, with other relevant officers (planning, conservation, street cleansing, and maintenance, for example) becoming involved as necessary.

To maintain inter-visibility which enables 'natural surveillance' and sight-lines to/from vehicles, trees should generally be planted with a minimum clear stem of 3m, increasing where necessary to avoid taller vehicles.



AVENUE / STREET TREE SPACING

Where trees are to be planted directly into the ground in the footway, verge, or median, they must be spaced to allow easy pedestrian movement between them. Generally this would be achieved by an 'air gap' of at least 1.2m at maturity unless a 'barrier' was the design intent. Spacings for the largest of street trees, like London Plane for example, may increase to around 20m. More typical spacings however will be around 6m (to co-ordinate with parking bays) and up to 12m, as few trees will be able to develop a canopy bigger than 12m in relatively impoverished street soil environments.

Townscape Character

This should have an influence on the types, size, and number of trees planted, particularly if they fall within a conservation area or are close to listed buildings. Planting strategies should be developed in these instances with the conservation officer and possibly local interest groups.

SPECIES

Scale, size + appropriateness

The scale of a public space is not simply a function of its size, and trees should be planted at a size, type, and spacing appropriate to their townscape context.

Form + Habit

Often trees with an upright, columnar or fastigate habit will be most useful for planting in streets adjacent to carriageways to avoid conflicts with vehicles, although larger trees with broad and spreading (and possibly weeping) habits are often also suitable - as the London Plane and Norway Maple demonstrate.

Mix

Species should generally be mixed to increase biodiversity and reduce the vulnerability of a single species to pest and disease attack, unless the design intent requires a single species. Mixes of between 3 - 5 species should be sufficient, although on bigger projects/sites more should be considered.

Proximity to buildings

Trees should generally not be planted within 3m of buildings unless they are very small with a compact columnar or fastigate habit, and known not to cause problems associated with shrinkable soils.

Proximity to signals

Trees must be placed to avoid blocking the sightline to a signal head, both at the time of planting and through to maturity. Generally, trees should not be planted with the nearest part of the trunk at maturity within 450mm from the face of the road kerb. To avoid obscuring a signal head a sufficient clear stem must be specified and maintained.

Light + shade

Trees are important in providing shade from the sun and shelter from the wind and rain. Some species of trees can be useful in deflecting light into shaded parts of a site. Care should be taken however, in the placement of trees to avoid blocking light into adjacent buildings.

Nursery stock sizes

In the public realm, clear stemmed trees smaller than Extra Heavy Standard (EHS) 18-20cm girth will be vulnerable to vandalism, particularly snapping of the leader. To try and prevent this, semi-mature trees starting at 20-25cm girth are preferred and should be the minimum size planted where it is anticipated that vandalism might be an issue.

CONSTRAINTS

Trees - Utilities & Underground Structures

Trees should not be planted directly on top of a known utility or underground structure unless it is sufficiently deep as not to be affected by the anticipated additional loading that will be caused by the tree at maturity.

Water and sewage pipes (if they leak) are known to attract roots which can cause additional damage. It may be necessary to protect such utilities with a root barrier. Modern plastic pipes and ducts will not normally be damaged by trees roots. Root barriers, therefore are not usually needed around these utilities. Underground structures such as basements and chambers will not normally be damaged by tree roots unless they are already fractured. Root barriers should therefore be considered around old structures which would be damaged by root ingress.

MAINTENANCE

Leaf litter.

Nearly all deciduous trees lose their leaves in the Autumn, which necessitates the collection of leaf litter by the maintaining authority. Keeping roads and pavements free of leaf litter is important for safety, particularly when wet, as failure to do so can result in slip hazards.

Certain tree species, for example Ginkgo and Ash, have a tendency to drop most of their leaves in a single cold snap, which can assist in effectively clearing the litter in a single session.



Green Infrastructure

TREE PITS + SuDS

Trees are a major asset to Lichfield and a vital component of its urban landscape. On streets and other hard landscaped areas within the public realm, value can be added by incorporating SuDS measures within new tree pits and trees within new or retrofitted SuDS measures. By combining trees with other SuDS components, the volume of rainwater interception and attenuation can be significantly increased, alongside improvements to water quality, amenity and biodiversity.

SuDS tree pits can accommodate a single tree or can be a series of connected pits, designed as part of a whole-street SuDS solution. Structural soils or proprietary crate systems create a structurally sound carriageway to accommodate traffic loads while allowing sufficient space below ground for the roots of healthy mature trees and the management of surface water run-off. Designs that propose a SuDS system under the carriageway must be approved by the Council's Highways Department.

Benefits

Environment: street trees manage pollution in city streets by filtering dust, wind and noise, contributing to urban cooling, providing shelter and by improving air quality.

Interception: trees intercept rainfall on their leaves. Some of this water drips to the ground, some evaporates. Tree roots also absorb water, which is either used by the tree or released into the atmosphere through evapotranspiration. This reduces the volume of water entering the drainage system.

Attenuation: tree pits can store storm water runoff within structural soils or proprietary crate systems.

Infiltration: soil infiltration rates are increased due to improved soil structure linked to root growth and associated living and decaying organic matter.

Filtration: soils and geotextiles that make up the construction of tree pits remove silts and particulates that may be present in runoff water. Through 'phytoremediation', trees absorb trace amounts of harmful chemicals – including metals, hydrocarbons and solvents – transforming them into less harmful substances or using them as nutrients.

Amenity: trees add colour and interest to the townscape, soften the visual impact of the built environment, and contribute greatly to the city's character. Tree lined streets make cycling and walking more attractive and therefore more popular, enhancing the health and wellbeing of Lichfield residents and visitors. The presence of trees can slow traffic by changing the scale of streets. All these

strategies have been incorporated into the main access routes into the city centre.

Biodiversity: trees constitute the largest element of biomass in the city, providing significant biodiversity value. Trees provide food, habitat and shelter for birds, invertebrates and other species. A large species tree, such as an oak, can host hundreds of different animals, plants and fungi, with long-term benefit to the urban ecology.

Considerations

Existing trees: these must be retained where possible, however providing new attenuation or infiltration areas around existing trees is rarely feasible without seriously harming them. Proposals relating to existing trees should accord with BS5837:2012 and take account of tree preservation orders and conservation area designations.

Available space: tree pits require space below ground to successfully accommodate long-term root growth. Tree pits and trenches (connected pits) should provide adequate soil volume,

Discharge/infiltration rate: this dictates the size of the tree pit required for water storage.

Irrigation: lack of water and nutrients kills newly planted trees. The design and maintenance regime should include a means of efficient irrigation.

Aeration: soils and roots need air to live. The design of the tree pit should provide an air supply below ground to facilitate gaseous exchange around the root system.

Utilities: the location of below ground services and drainage should be identified to ensure root zones, utilities, and other below ground infrastructure are all coordinated. Protection for both long-term root growth and below ground infrastructure can be provided with root barriers.

Tree specification: considerations include tree species and diversity, provenance, mature size, clear stem height, root preparation and procurement.

Soil: the depth and type of soil should be appropriate for the tree species. Excessive topsoil depth increases the risk of anaerobic conditions (oxygen deficiency), which can affect the health of the tree. Topsoil should therefore only be used within the upper part of the soil profile, with suitable subsoil in the lower layer. Depths will be dependent on soil conditions, the tree specification and the type of load-bearing system employed.

Loading: the design and specification of the tree pit should take account of vertical loads imposed by traffic above and

from lateral loads imposed on the sides of the structure.

Pollution/contamination: pollution and contamination sources affecting surface and ground water can influence tree growth. Certain tree species are more susceptible than others, so species selection should be specific to each site and catchment area.

Inlets: surface water can be introduced through channels or rills as direct surface water runoff to a tree pit; via depressions or low points directing runoff from impermeable surfaces towards the tree pit; or via permeable surfaces used to collect and convey surface water to the tree pit.

Outlets: waterlogging can be a key reason for failure, so tree pits should be well drained. This is best achieved by infiltration if ground conditions are suitable. Where infiltration is not possible then an outfall to a surface water drainage network can be used.

MAINTENANCE

Trees require a higher level of management during the first five years after planting because roots need to establish good contact with the growing medium before they can efficiently extract water. Maintenance regimes for tree pits are likely to include irrigation, removal of leaf litter, staking and tying, formative pruning and crown lifting, changes to materials around the base of the tree (e.g. tree grilles, grates, permeable paving), and clearing debris from inlets and outlets.

THE ROOTING ZONE

GENERAL CONSIDERATIONS

The rooting zone is the area surrounding the tree pit into which the tree roots can spread as the tree develops. Ideally it should surround the tree equally on all sides to the anticipated edge of the canopy at maturity. Whilst underground constraints will make this difficult, the size should be maximised. Rooting zones can also be shared by adjacent trees and benefit from being linked together where possible.

Approximate Volumes

Trees were traditionally planted in pits without constructed/artificial root zones. Roots would find their way out of the pit into the adjacent soils and sub-soils. But then underground conditions previously were generally much less compromised than they are today - for example, without impermeable pavements, compacted and contaminated soils, and utilities. Planned and constructed root zones are therefore now almost essential for trees in hard paved areas. If no root zone can be provided due

to underground constraints the tree pit should be made as large as possible, and species selected to tolerate the impoverished conditions. So called 'Pioneer' species, such as Birch and Alder, may be particularly suited to these conditions and may survive and thrive even without a constructed root-zone.

As a minimum trees in the street require a well prepared and specified soil volume for satisfactory establishment and growth. Generally the root zone should extend as far as possible to the anticipated canopy edge of the mature tree at a depth of between 600 and 900mm below the pavement construction (i.e. up to a maximum of 1.2m total depth to account for pavement construction and drainage layer) to approximate natural soil profiles.

Although these requirements differ for various species and varieties of tree, as a rough guide the minimum recommended soil volumes are:

Small tree: 5-15 cubic metres

Medium tree: 20 - 40 cubic metres

Large tree: 50 cubic metres



Cellular root zone protection

Green Infrastructure

SuDS PLANTING

RAINGARDENS

Rain gardens are simply shrubs beds (although often planted with grasses) that have been designed to receive surface water from pedestrian and vehicle surfaces, or roof water run-off. Raingardens should be considered in all areas where shrubs, grasses, or lawns are proposed, in local surface water catchment areas and where underground conditions allow. In all of these instances, raingardens should also be considered for supplementary tree planting. Where the aspiration is to create a boulevard effect along a road corridor such as The Friary and Birmingham Road would be ideal location for raingardens in conjunction with avenue tree planting.

Species mix

Planting can range from wildflower mixes, grass mixes to low maintenance shrub mixes although mown grass verges / lawns may also be considered.

Planting medium

This needs to be carefully designed to provide sufficient nutrients to support plant growth and be free draining enough to allow water to infiltrate. Low nutrient growing mediums should therefore be considered to reduce the need for weeding.

Drainage

This will depend on the nature of the subsoils and the quantity of water anticipated. Piped overflow drains may there be required.

Mulch

Depending on the growing medium and species mix an 80mm depth maybe required during the establishment period to suppress weeds and retain moisture.



MAINTENANCE

Weeding

During the establishment period and until a closed canopy can suppress weed growth, weeding will need to be carried out at approximately monthly intervals through the growing season.

Watering

Watering may need to be carried out, especially in the establishment period and in periods of dry weather, depending on the species mix and planting medium for example.

Feeding

Spring and autumn feeds are likely to be necessary, depending on the species mix and soil medium.

Pruning

Pruning is likely to be necessary, depending on the species mix and soil medium.

De-silting / cleaning

SuDS beds, over time may become silted up which may impact on plant growth and/or their efficacy as SuDS components. In such cases it is likely the plants will need to be lifted, the drainage and growing mediums replaced, and then replanted. Any sacrificial component, designed to collect oils and other pollutants for example, will need to be replaced when they have reached saturation point.

Examples of raingardens



GREEN WALLS & ROOFS

Roofs and walls can provide the first point of interception as components of the SuDS management process. They are an effective and visually appealing way to integrate green infrastructure, even in tight, densely developed areas. A number of the alleyways, for example, Lloyd’s Walk, would be suitable locations to establish green walls.

Living roofs and walls can be designed as an integral part of new structures or retrofitted to existing structures.



As discussed earlier in the report, the historic, narrow nature of the streets in the city core is not conducive to extensive tree planting and city greening. Therefore every opportunity should be taken to include green roofs within new development.

Benefits

Attenuation: living roofs and walls can be used to intercept and attenuate rainwater. They allow a reduced discharge rate through evaporation and transpiration.

Filtration: living structures treat water through a variety of physical, biological and chemical processes within the soil and root uptake zones. They regulate surface water runoff temperature that could otherwise adversely affect the ecology of local water bodies.

Amenity: the aesthetics of a structure can be improved, softening the hard urban environment. Living structures can reduce air temperatures and can also act as a learning and urban farming resource, as plants on green roofs and walls can be used for growing food.

Biodiversity: living roofs and walls safeguard, enhance, restore, and create habitat with no additional land take. They provide important habitat stepping stones and contribute to the city’s natural capital. In particular, they provide refuge for invertebrates and food for pollinators.

Considerations

Substrate: green roofs can be designed with a variety of substrate materials and depths. Growing media can be soil, recycled materials, dead wood and aggregates. It is possible to choose more than one substrate on a single roof to create different microclimates and accommodate greater habitat diversity.

Vegetation: Plants can be seeded, self-seeded or pre-grown and planted. Species selection should be adapted to the microclimate and substrate specifications. Roof conditions can be hostile, with high winds, extreme temperatures, high rainfall and drought. Diverse dry meadow mixes, naturally self-sustaining in exposed environments, are a viable option. Natural windblown or bird-borne self-seeding is an economic alternative and will result in a naturally adapted selection of plants.

Structural resilience: living roofs add loading to a structure. Dead loads vary depending on the material used but are typically around 0.7-5.0 kN/m. Imposed loads can be up to 10 kN/m.

Irrigation: rainwater should be intercepted for irrigation, where possible. In some circumstances supplementary irrigation may be required to maintain vegetation.

Exceedance: the design of the green roof should be able to accommodate excessive rainfall by providing a suitable outfall.

Fire resistance: Fire risks can be managed using appropriate materials and design. Vegetation should be kept away from vulnerable areas such as PV panels and technology for example.

Access, safety and edge protection: Outlets and drains should be easily and safely accessible for inspection.

Green Infrastructure

MAINTENANCE

Periodic maintenance will include irrigation, inspection of outlets, and removal of invasive/unwanted plants. Green walls formed by climbing plants may need to be attached to supports as they grow. Proprietary products require maintenance of plants and irrigation systems, and may need occasional replanting.

SUSTAINABLE URBAN DRAINAGE SYSTEMS (SuDS)

Public realm projects within the city must contribute to Lichfield's ambitions to improve surface water management, and mitigate the risks of associated flooding, through delivering SuDS and water attenuation, in combination with trees and planting wherever possible.

The impact of climate change and the consequence of flooding is more significant around our towns and cities for a number of reasons:

- The air can be warmer, due to the heat that we generate during our day-to-day activities such as travelling around, manufacturing goods or heating our homes. This is known as the Urban Heat Island effect and results in more frequent higher intensity weather.
- Paving, or building, over areas which previously absorbed water means that rainfall runs off the surfaces much more quickly and enters the drainage system over a much shorter period of time. These man-made surfaces are also often dark in colour and absorb heat, again adding to the Urban Heat Island effect.

As a result, the capacity of our drainage system is being put under more pressure, and the consequences of flood events are becoming more severe,

SuDS are a crucial tool to mitigate flooding and managing the risk of pollution in a sustainable and cost-effective way. They can help manage the quality of runoff to protect the natural environment from pollution by treating or filtering the water before it enters the drainage network. This treatment can include the removal of sediment, silts and fine particulates or spillages of contaminants such as oils.

SuDS are generally made up of a sequence of components that manage the quantity and quality of water which runs off hard and man-made surfaces. Ideally the water should be managed from its source (the location where the rain lands on the surface) to the point at which it is discharged to the receiving watercourse or sewer.

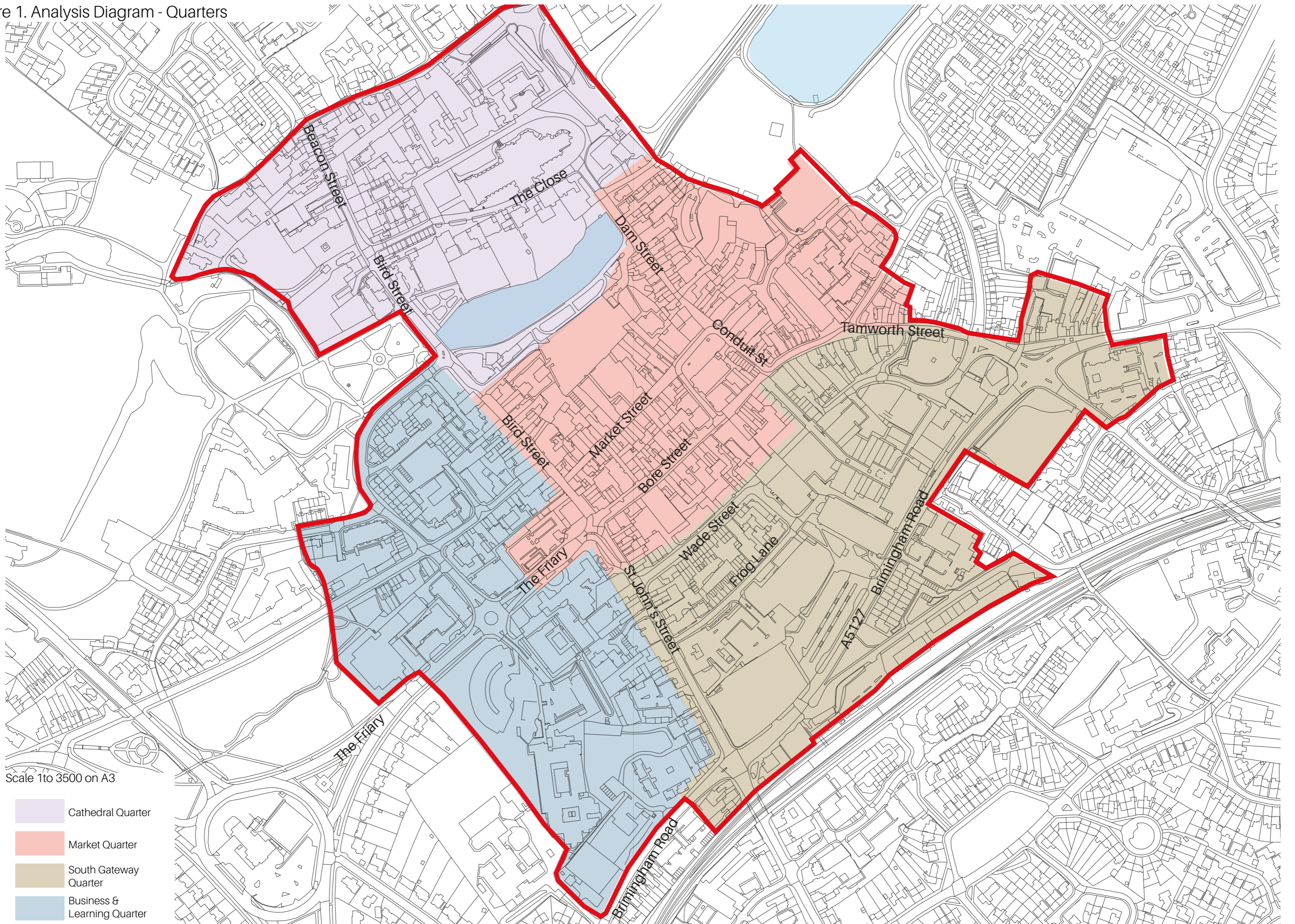
Managing the water from source to receptor will normally require a number of SuDS components to work in sequence to collect, store, convey and treat the water;

CIRIA calls this sequence the 'SuDS management train' and this terminology is widely used in the water management industry.

In the city centre of Lichfield the most appropriate SuDS measures would be raingardens and the use of permeable paving and materials.

These measures also contribute positively to the amenity and biodiversity of spaces. Water quantity, water quality, amenity and biodiversity are referred to as the four pillars of SuDS design, as set out in The SuDS Manual, Ciria C753, 2015.

Figure 1. Analysis Diagram - Quarters



Scale 1to 3500 on A3

- Cathedral Quarter
- Market Quarter
- South Gateway Quarter
- Business & Learning Quarter

Figure 2. Analysis Diagram - Ownerships

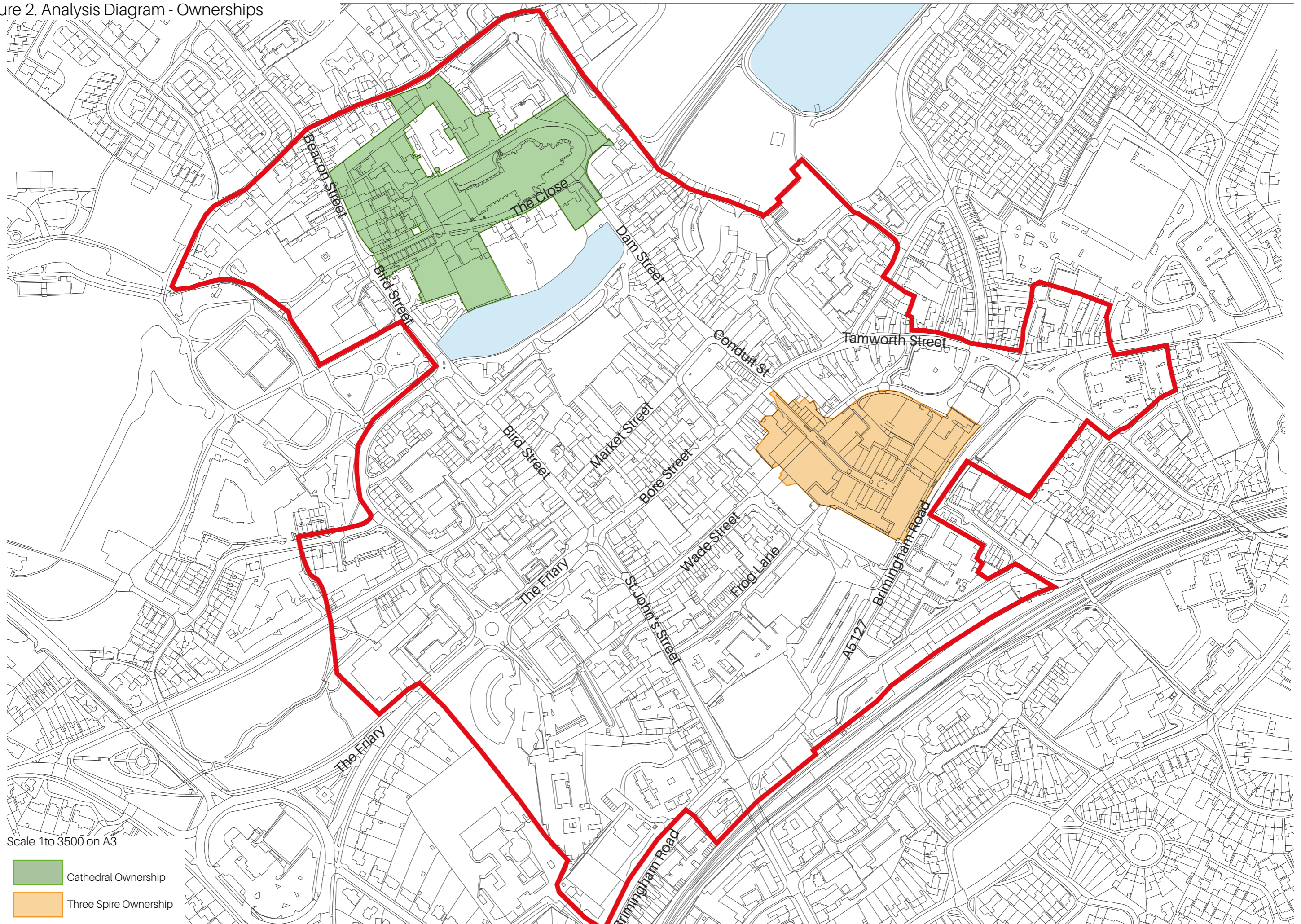


Figure 3. Analysis Diagram - TRO

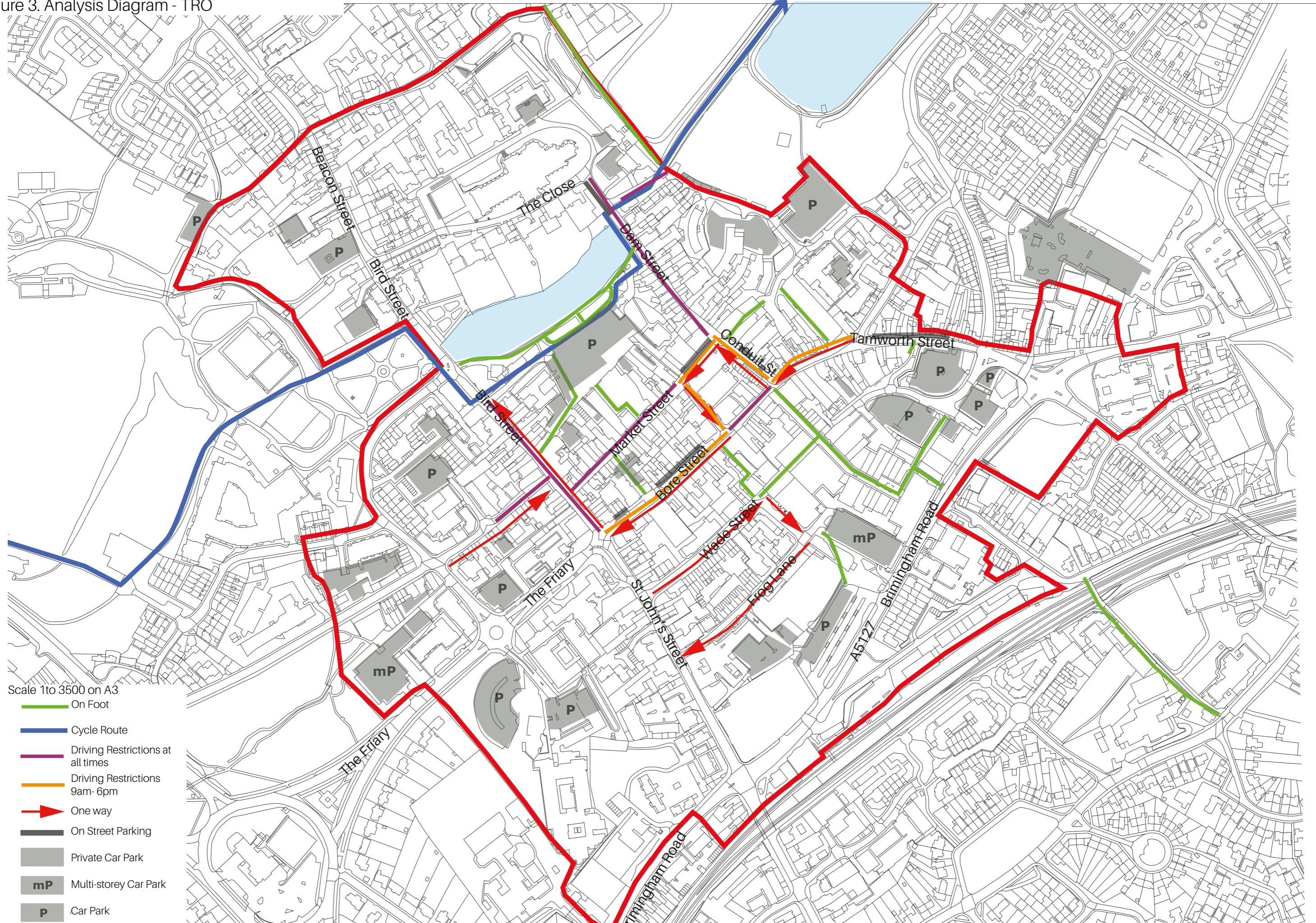


Figure 4. Analysis Diagram - Trails

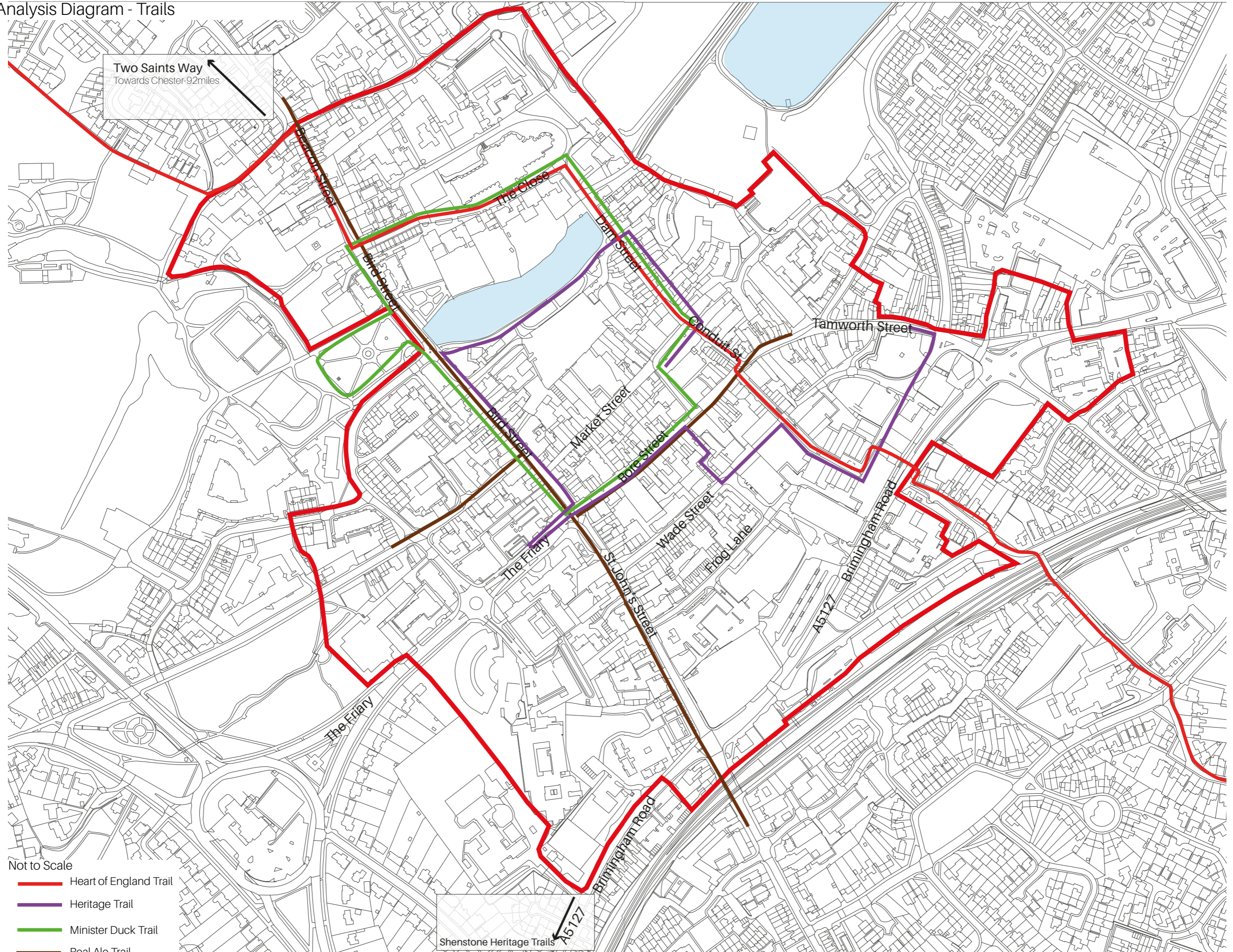


Figure 5. Public Realm Projects

Keys12

Market Quarter

- 1. Market Square/
Breadmarket St/Bore St/
Conduit St/Tamworth St
- 2. Bore Street East
- 3. Market Street
- 4. Bird Street Gateway
- 5. Bird Street/ Sandford
Street/Car Park entrance
- 6. Dam Street
- 7. Lloyd's Walk
- 8. Minster Pool Walk/
Gardens
- 9. Reeve Lane

Southern Gateway Quarter

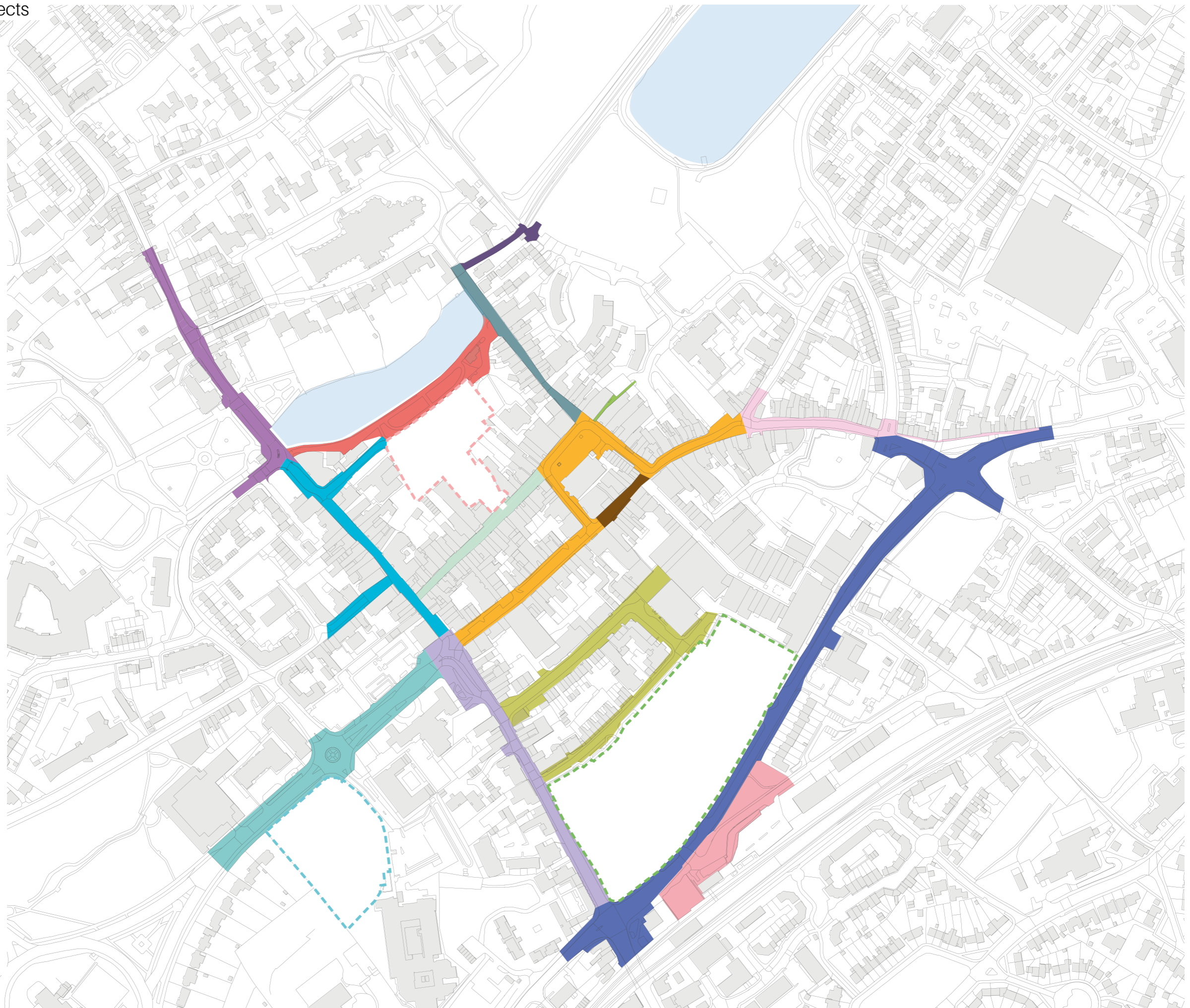
- 10. Wade Street/Castle
Dyke/Frog Lane
- 11. Birmingham Road/
Greenhill Jctn/St Johns
St Jctn
- 12. St Johns Street
- 13. Station Square
- 14. Tamworth Street/
Lombard St/George Ln
(parts)

Business & Learning Quarter

- 15. The Friary

Development Sites

- Bird Street Car Park
Development Site
- Birmingham Road
Development Site/
District Council House
- University Development
Site



Scale 1 to 3500 on A3

Figure 6. Suggested Order of Project Priority (see Cost Summary pg 92)

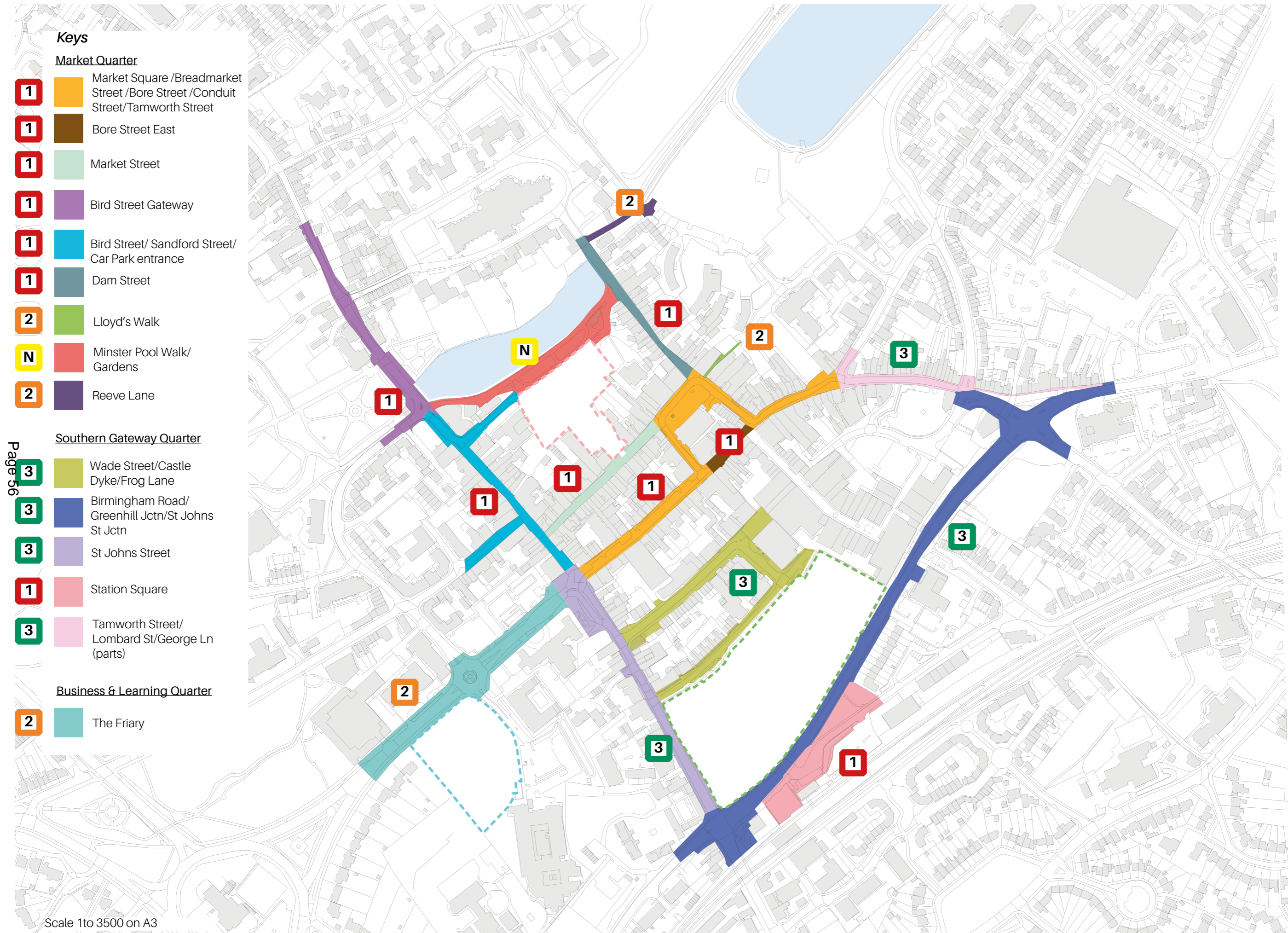
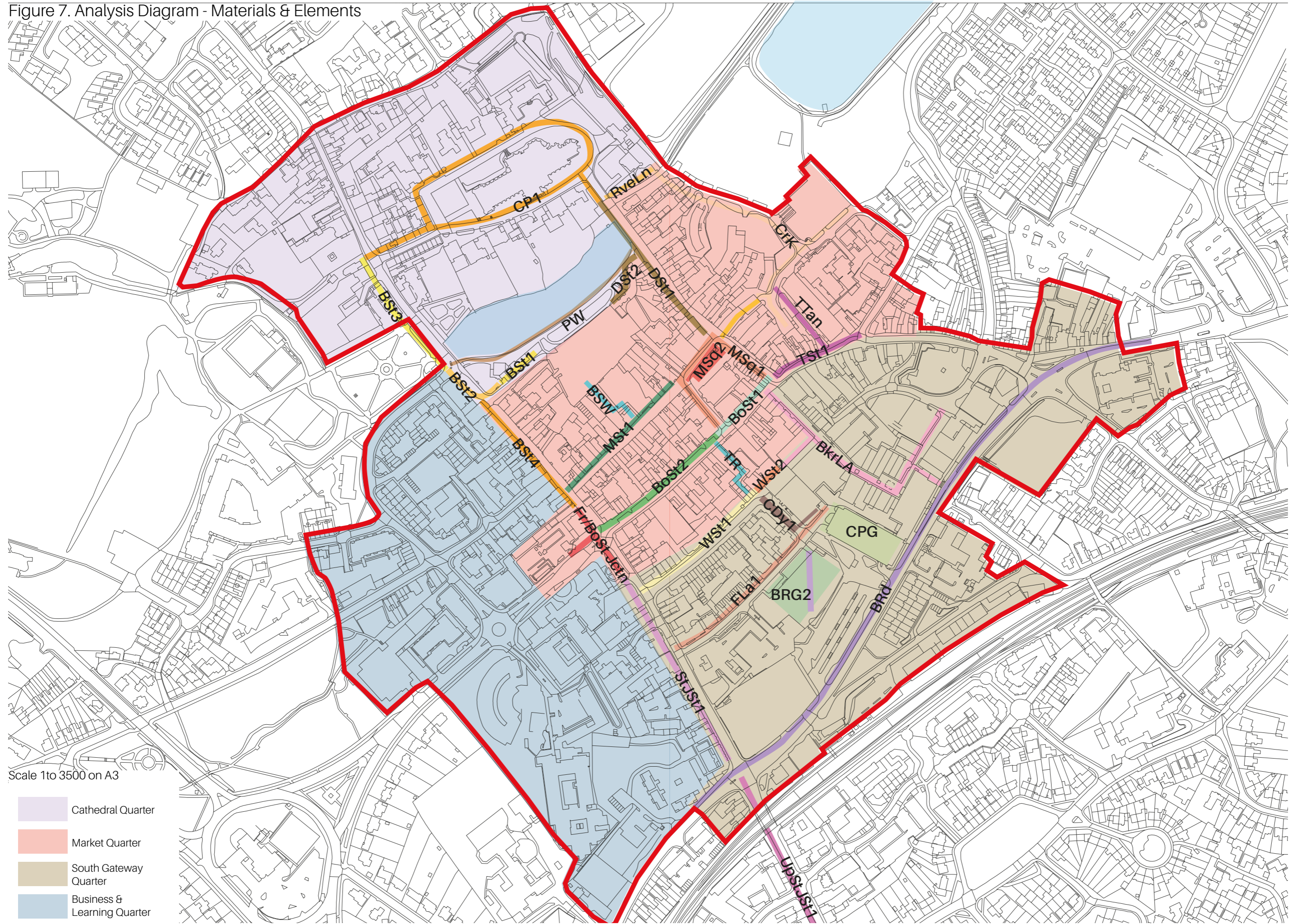


Figure 7. Analysis Diagram - Materials & Elements



Scale 1to 3500 on A3

- Cathedral Quarter
- Market Quarter
- South Gateway Quarter
- Business & Learning Quarter

Materials Palette Summary

LICHFIELD - PUBLIC REALM MATERIALS

Cathedral Quarter

Street	Drawing Code	Area	Description	Comments
Cathedral Precinct	CP1	Highway	Tarmac	
		Footpath	Yorkstone Flags	
		Kerbs	Yorkstone	Section of new kerb has been laid in granite to front of proposed sculpture location.
		Channel	Cobble and setts	
		Back of paving/infill	Cobble on edge detail	
		Bollards	Square timber with chain	
			Round timber vehicular deterrent	
			Heritage black steel vehicular bollards	
		Railings/handrails	Light, black steel	Light and elegant design.
		Lighting	Black steel faux gas light medium height fittings, with ladder bars	
Pool Walk	PW	Footpath	Tarmac	
		Seating areas	PCC flags	
		Tree pits	Asphalt with concrete flag surrounds	
		Railings	Black decorative vertical bar rail to pool edge.	Heritage detailing
		Seats	metal scroll arm benches, steel slats	Lichfield Green
		Lighting	Uplighters within tree pits	
		Strands of lights between trees.		
		Downlighters within trees		
Bird Street North	BST3	Highway	Tarmac	
		Footpath (East)	PCC Flags	
		Footpath (west)	Tarmac	
		Kerbs	Concrete	

Materials and Elements - Cathedral Quarter

CATHEDRAL PRECINCT- CP1



Highway - Tarmac



Footpath - Yorkstone Flags



Kerbs - Yorkstone



Channel - Cobble and setts



Back of paving/infill - Cobble on edge detail



Bollards - Heritage black steel vehicular bollards



Bollards - Square timber with chain



Bollards - Round timber vehicular deterrent



Railings/handrails - Light, black steel



Railings/handrails - Light, black steel



Lighting - Black steel faux gas light medium height fittings, with ladder bars

Materials and Elements - Cathedral Quarter

POOL WALK - PW



Footpath - Tarmac



Seating Area - PCC flags



Tree pit - Asphalt with concrete flag surrounds



Railings - Black decorative vertical bar rail to pool edge.



Seating - metal scroll arm benches, steel slats



Bollards - Heritage black steel vehicular bollards



Lighting - Uplighters within tree pits, Downlighters within trees Strands of lights between trees.



BIRD STREET NORTH- BST3



Highway - Tarmac



Footpath East - PCC flags



Footpath West - Tarmac



Kerb - Concrete



Materials Palette Summary

LICHFIELD - PUBLIC REALM MATERIALS

Market Quarter

Street	Drawing Code	Area	Description	Comments		
Dam Street	DSt1	Highway	Red Brick (herringbone)	wall to wall		
		Channel	Red Brick	bricks around channel and manholes in poor condition		
		Tree pits	circular steel grilles	lifting		
Reeve Lane	RveLn	Highway	Tarmac	wall to wall		
		Bollards	Steel (black) mix of sizes and type			
		Lighting	Medium height contemporary street lighting			
Dam Street to Bird Street Car Park	DSt2	Footpath	Red brick (herringbone)			
		Edging	PCC flags			
		Tree pits	Basalt setts circle			
		Bollards	Black steel heritage bollards	Heritage detailing		
		Seats	Metal scroll arm benches, timber slats (heritage)	Black		
Bird Street	BSt1	Highway	Blue eng brick (herringbone)			
		Footpath	Tarmac			
		Lighting	Wall-mounted highway			
		Kerbs	Concrete			
		Tree pits	circular steel grilles, black basalt sett trim, black steel tree guard	Pits have sunk		
Bird Street	BSt2	Highway	Blue eng brick (herringbone)			
		Channel	Blue eng brick (running)			
		Footpath	Red eng brick (stretcher)			
		Footpath	Yorkstone to west of junction			
		Kerbs	Concrete			
		Seating	Metal scroll arm benches, steel slats	Lichfield Green		
		Lighting	Contemporary highways column			
		Bollards	Square section timber	poor condition		
Bird Street	BSt4	Highway	Blue eng brick (herringbone)			
		Footpath	Red eng brick (basketweave)			
		Footpath	Intermittent stretches of YorkStone			
		Channel	Blue eng brick dished channel			
		Seating	Steel scroll arm steel slats	Lichfield Green		
		Bollards	Square section timber	poor condition		
		Lighting	Building mounted			
			infill paving at building edge	Cobble on edge detail		
		Bird Street/Swan Road Jcn		Highway	Grey concrete block	
		Market Street	MS1	Highway	Buff concrete paviours (herringbone)	
Channel	Buff concrete paviours (running)			To north of road		
Lighting	Building mounted					
Bollards	Lack steel heritage bollards					
Seating	decorative black steel ends, timber slats					
	Steel scroll arm steel slats			Lichfield Green		
Tree pits	Square, concrete pin kerbs					
Bird Street Walk	BSW	Footpath	Red brick stretcher bond with blue brick stretcher bond edge			
		Lighting	Wall mounted			
Market Square	MSq1	Highway	Multi red concrete paviour			
		Kerbs	Red granite	75mm high		
		Footpath	Yorkstone flags			
		Channel	Black basalt sett			
		Lighting	Building mounted			
		Parking Bays	Delineated in black basalt setts			
			MSq2	Paving	Buff multi concrete paviours	
				Bands	Yorkstone flags with red granite trims	set off church buttresses
				Trims	Black basalt sett	
				Infill at building edge	Cobble on edge detail	
		Channel	Multi rectangular granite setts			
		Tree pits	York stone quartile with round opening, circular steel tree guard	Set within bands		
		Seating	Steel scroll arm steel slats	Lichfield green		

Materials and elements - Market Quarter

Materials and Elements - Market Quarter

DAM STREET- DST1



Highway - Red Brick (Herringbone)



Channel - Red Brick



Tree Pit - Circular steel grilles

REEVE LANE- RVELN



Tarmac - Highway



Bollards - Steel (black) mixed size/type



Lighting - Medium height contemporary street lighting

BIRD STREET- BST3



Highway - Blue eng brick

BIRD STREET- BST2



Highway - Blue eng brick



Channel - Blue eng brick (running)



Footpath - Red eng brick (stretcher)

Page 60

DAM STREET TO BIRD STREET CAR PARK- DST2



Footpath - Red Brick (Herringbone)



Edging - PCC flags



Tree Pit - Basalt setts circle

BIRD STREET- BST4



Footpath - Yorkstone to west of junction



Bollards - Square timber



Bollards - Black steel heritage



Seating - Metal scroll arm benches, steel slats



Highway - Blue eng brick



Footpath - Red eng brick (basket weave)



Channel - Blue eng brick (running)

BIRD STREET- BST4 CONT...



Bollards - Square timber



Infill paving at building edge - Cobble on edge detail

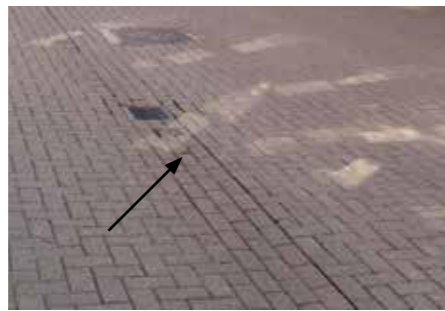


Bird Street/Swan Road Jnctn - Grey concrete block

MARKET STREET- MST1



Highway - Buff concrete paviers (herringbone)



Channel - Buff concrete paviers (running)



Lighting - Building mounted

Page 61



Bollards - Black steel heritage bollards



Seating - Steel scroll arm steel slats



Decorative black steel ends, timber slats



Tree Pit - Square, concrete pin kerbs

BIRD STREET WALK- BSW



Footway - Red block stretcher bond with blue block stretcher bond edge

MARKET SQUARE- MSQ1



Highway - Multi red concrete paviour



Kerbs - Red Granite



Footway - Buff concrete paviers

MARKET SQUARE- MSQ2



Lighting - Building mounted



Parking bays - Delineated in black basalt setts



Paving - Buff multi concrete paviers



Bands - Yorkstone flags with red granite trims

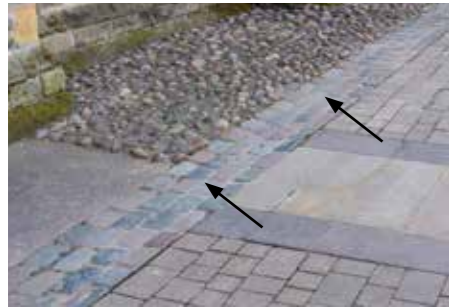


Trims - Black basalt sett



Infill paving at building edge - Cobble on edge detail

MARKET SQUARE- MSQ2 CONT



Channel - Multi rectangular granite setts



Tree Pit - York stone quartile w/ round opening, circular steel tree guard



Seating - Steel scroll arm steel slats



Bollards - Contemporary black steel, fluorescent band to top



Lighting - Building mounted



Lighting - Uplighters to sculptures

BORE STREET- BOST1



Highway - Concrete paviours (herringbone)



Footpath - PCC flags within arcade



Channel - Concrete paviours (stretcher) central

BORE STREET- BOST2



Highway - Tumbled concrete paviours



Footpath - Yorkstone flags



Channel - Black basalt sett

BORE STREET- BOST2 CONT...



Kerbs - Red granite 100mm high



Parking Bays - Black basalt setts



Crossovers - Yorkstone Setts

FRIARY/BORE STREET- FR/BOST JNCTN



Highway - Tarmac



Crossovers - Yorkstone Setts



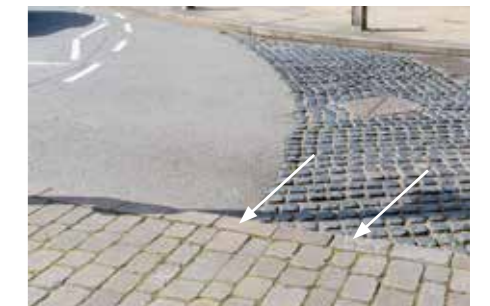
Footpath - Yorkstone



Footpaths - Red brick (stretcher) to arcade



Footpaths - PCC flags to south west



Kerbs - Yorkstone inner kerbs



Kerbs - St John's transitions to full height concrete



Bollards - Square section timber



Seating - Steel scroll arm steel slats

Materials and Elements - Market Quarter

FRIARY/BORE STREET- FR/BOST JNCTN CONT...



Tree Pit - Circular basalt sett trim

TAMWORTH STREET- TST1



Highway - Concrete tegula paving multi



Highway - Transitions yorkstone setts



Footpaths - Yorkstone Flags



Kerbs - Red granite transitions from 75-150mm high



Channel - Black basalt sett

THE TANNERIES- TTAN



Footpath - Tarmac



Kerbs - Flush concrete pin kerb and gravel

LICHFIELD - PUBLIC REALM MATERIALS

Southern Gateway

Street	Drawing Code	Area	Description	Comments		
Wade Street	WS1	Highway	Tarmac			
		Footpath	400 x 400 concrete slabs			
		Kerbs	Concrete			
		Crossovers	Blue eng brick (grooved)			
		Back of paving/infill	Cobble on edge detail	Poorly laid - not enough cobbles to mortar		
		Lighting	Highways lighting columns			
		Bollards	Square timber section	Poor condition		
		WS2	Paving	Concrete pavours with granite sett banding		
			Lighting	Highways lighting columns		
				Uplighters to Garrick Theatre		
Seating	Steel scroll arm with steel slats		Lichfield Green			
Castle Dyke	CDy1	Highway	Tarmac			
		Footpath	400 x 400 concrete slabs, red brick banding detail			
		Kerbs	Concrete			
		Bollards	Square timber section	Poor condition		
		Tree pits	Circular steel tree grilles, grey concrete block edging			
Frog Lane	FLa1	Highway	Tarmac			
		Footpath	400 x 400 concrete slabs (north)			
			Tarmac (south)			
		Kerbs	Concrete			
		Crossovers	Blue eng brick (grooved)			
		Back of paving/infill	Cobble on edge detail	Poorly laid		
		Bollards	Square timber section	Poor condition		
		Lighting	Highways lighting columns			
		St John's Street	StJSt1	Highway	Tarmac	
				Kerbs	Concrete	
Footpaths	400 x 400 buff PCC flags					
Baker's Lane	BkrLa	Paving	Red brick blocks set in buff concrete flags with blue and cream coloured brick trims. Blocks of brown concrete pavors set in			
		Tree pits	Raised square brick planters with gravel fill			
		Lighting	Contemporary 8m column lighting with hanging baskets, gold trim and banner mountings			
		Seating	Black steel ended benches with timber slats			
		Railings	Contemporary system railings	heavy in appearance		
Upper St John's Street	UpStJSt1	Highway	Tarmac			
		Kerbs	Concrete			
		Footpaths	Tarmac			
		Lighting	10m highway lighting columns			
Birmingham Road	BRd	Higway	Tarmac			
		Kerbs	Concrete			
		Footpaths	Tarmac			
		Lighting	10m highway lighting columns			

Materials and Elements - Southern Gateway Quarter

Materials and Elements - Southern Gateway Quarter

WADE STREET- WST1



Highway - Tarmac



Footpaths - 400 x 400 concrete slabs



Kerbs - Concrete



Crossovers - Blue eng brick (grooved)



Infill paving at building edge - Cobble on edge detail



Lighting - Highways lighting columns



Bollards - Square timber section

Page 64

WADE STREET- WST2



Paving - Concrete paviours with granite sett banding



Lighting - Highways lighting columns



Seating - Steel scroll arm steel slats

WADE STREET- WST2 CONT...



Cycle racks - Stainless steel Sheffield hoops



Channel - Aco drain denotes roadline

CASTLE DYKE- CDY1



Highway - Tarmac



Footpaths - 400 x 400 concrete slabs, red brick banding detail



Kerbs - Concrete



Bollards - Square timber section



Tree Pit - Circular steel tree grilles, grey concrete block edging

FROG LANE- FLA1



Highway - Tarmac



Footpaths - 400 x 400 concrete slabs (north)



Footpaths - Tarmac (south)

Materials and Elements - Southern Gateway Quarter

Materials and Elements - Southern Gateway Quarter

FROG LANE- FLA1 CONT...



Kerbs - Concrete



Crossovers - Blue eng brick (grooved)



Infill paving at building edge - Cobble on edge detail



Bollards - Square timber section



Bollards - Lichfield Green steel heritage bollards



Lighting - Highways lighting columns

BAKER'S LANE- BKRLA CONT...



Seating - Black steel ended benches with timber slats (Google Image)



Railings - Contemporary system railings



Railings - Contemporary system railings

UPPER ST JOHN'S STREET- UPSTJST1



Highway - Tarmac



Kerbs - Concrete



Footpaths - Tarmac

Page 65

ST JOHN'S STREET- STJST1



Highway - Tarmac



Kerbs - Concrete



Footpaths - 400 x 400 buff PCC flags



Lighting - 10m highway lighting columns



Seating - Steel scroll arm steel slats

BAKER'S LANE- BKRLA



Paving - Red brick blocks set in buff concrete flags with blue and cream coloured brick trims. Blocks of brown concrete paviors set in various coloured brick trims



Tree Pit - Raised square brick planters with gravel fill



Lighting - Contemporary 6m column lighting with hanging baskets, gold trim and banner mountings

Materials and Elements - Southern Gateway Quarter

BIRMINGHAM ROAD- BRD



Highway - Tarmac



Kerbs - Concrete



Footpaths - Tarmac



Lighting - 10m highway lighting columns



Lighting - 10m highway lighting columns

Order of Cost

Lichfield District Council
Public Realm Strategy
Priority and Order of Cost Summary



Priority 1	Proj Ref	Comments	Project Rationale	Area m2	All in Rate £	Order of Cost £
Timescale		3 years				
Aims/Benefits		<p>Improve the night-time economy</p> <p>Unify, enhance and consolidate the core historic public streets and spaces around Market Square</p> <p>Address aging and failing public realm</p> <p>Embrace and present heritage</p> <p>Encourage footfall/dwell time</p> <p>Signpost move to active travel principles and create key public transport gateway</p>				
Projects	1	Market Square/Breadmarket Street/Bore Street/Conduit Street/Tamworth Street	Retain the quality existing paving within the historic city core area, but for consistency, implement the lighting, signage, and street furniture in line with the strategy.	6,900	£31.59	£218,000
	3	Market Street	A busy retail street, connecting Market Square with the main food and drink offer along Bird Street. Also the point of connection to an improved Bird Street Walk and onto Minster Pool, Market Street is a key component of the proposed central wayfinding route and is therefore, a high priority for improvement.	1,495	£294.31	£440,000
	4	Bird Street Gateway	This is a priority project as it addresses a number of movement issues around the Swan Road/Bird Street junction - extending the green corridor from Beacon Park diagonally to Minster Pool; from Minster Pool to The Close and into the Cathedral Precinct; and connecting the café and restaurant offer on Bird Street to both the park and the Cathedral.	3,785	£321.53	£1,217,000
	5	Bird Street/Sandford Street/Car Park entrance	Bird Street, with its many restaurants, pubs and bars is a significant part of the night time economy in Lichfield, and a key attraction for residents, visitors and potential investors. We obviously wish to build on this and want to ensure that the area is as welcoming and visually pleasing as possible. The current paving in the area is suffering significant degradation, detracting from the attractiveness of the area. Bird Street is identified, therefore, as a top priority, as its delivery will meet the aims and objectives for Priority 1 projects	3,712	£306.30	£1,141,000
	2	Bore Street (east)	The poor quality of the existing materials in this short stretch of the historic core, is highlighted by, and detracts from the high quality of the surrounding paving. For completeness, address this area as a high priority.	598	£324.41	£194,000

Lichfield District Council
Public Realm Strategy
Priority and Order of Cost Summary



Priority 1	Proj Ref		Comments	Project Rationale	Area m2	All in Rate £	Order of Cost £
Projects	6	Dam Street		A high priority project that would involve repairing the eastern rail of the historic ladder street pattern running north from Market Square and the main connection to the Cathedral (until such time as the Bird Street Car Park works are realised). The project also addresses the connections from Minster Pool to Reeve Lane and onto Stowe Pool and National Cycle Route, and in conjunction with the improvements to Bird Street would complete the revised Minster Pool Loop into the Cathedral precinct.	1,644	£341.85	£562,000
	13	Station Square		The importance of this key gateway space in front of the station as a welcome to the visitor and traveller is fundamental to what we are trying to achieve within the city. The message of a fresh focus on urban quality, active travel and public transport, must start here, so a quality public realm, ease of movement and clear wayfinding is critical. Although removed from the city core, the importance of the area merits a Priority 1 status.	2,906	£532.35	£2,171,000
			City wide Lighting Strategy		1	item	£80,000
			City wide Public Art Strategy		1	item	£25,000
			City Wide Signage and Wayfinding Strategy		1	item	£30,000
Priority 2	Proj Ref		Comments	Project Rationale	Area m2	All in Rate £	Order of Cost £
Timescale		5 years					
Aims/Benefits		Begin to define distinct Quarter character Improve key connectivity and links to city centre Promote active travel Increase Green Infrastructure					
Projects	15	The Friary		In a similar vein to Lloyd's Walk, the proposals to enhance The Friary for pedestrians and cyclists would be determined as a Priority 2 project, improving connections to the underused Friary Car Park, thereby encouraging its use and allowing traffic to be removed or restricted within the city centre areas. The creation of a tree boulevard character would also accord with the spirit of the Business and Learning Quarter.	6,616	£781.29	£5,129,000
	7	Lloyd's Walk		With the move to restrict car and vehicular movements around the city centre and encourage walking and cycling, the proposals seek to improve the active travel network and encouraging the use of the edge of centre car parks. Pedestrian access to these car parks is, therefore, critical and addressing the perceptions of poor personal security and improving the sense of welcome along Lloyd's Walk would further the aims of a Priority 2 project.	137	£525.55	£72,000
	9	Reeve Lane		Reeve Lane would extend the green corridor running west/east through the city from Beacon Park across Minster Pool Walk to Stowe Pool. Connecting the open space of Stowe Pool to the city and cathedral precinct would achieve the connectivity aims and objectives for Priority 2 projects.	1,832	£390.28	£255,000

Lichfield District Council
Public Realm Strategy
Priority and Order of Cost Summary




Page 69

Priority 3	Proj Ref		Comments	Project Rationale	Area m2	All in Rate £	Order of Cost £
Timescale		10 years					
Aims/Benefits		Complete active travel network Integrate new development Consolidate city-wide public realm Establish Quarter identities					
Projects	10	Wade Street/Castle Dyke/ Frog Lane	*St Johns Street and Birmingham Road may be undertaken in conjunction with the delivery of the Southern Gateway development.	This predominantly residential area also provides access to commercial and business properties, a role that would increase with the development of the Southern Gateway scheme. Castle Dyke would also become a shared surface creating a much more generous frontage to the Garrick Theatre and incident of public realm along the central wayfinding route.	4,156	£302.21	£1,989,000
	14	Tamworth Street/Greenhill		An important vehicular route into the city from Birmingham Road providing access to Lombard Car Park. The historic street is narrow, with restricted pavements and extensive highway infrastructure around car parks and junctions. The proposal to narrow carriageways whilst retaining useful short-term, on-street parking would free up space for the pedestrian. A rationalisation of highway furniture and infrastructure would also maintain the scale of the street and remove clutter. The project aligns with the aims and benefits of a Priority 3 project to improve connectivity and wayfinding.	2,319	£310.05	£719,000
	12	St Johns Street*		Narrow pavements and heavy traffic epitomise this route into the city centre. The street also provides the setting for the listed Hospital of Saint John the Baptist and the Council Offices buildings and garden. With the development of the Southern Gateway site, links to the station and promotion of active travel routes, space for the pedestrian and cyclist must be found. The project is designated as Priority 3, improving connectivity around the city, but options are tabled to restrict traffic along this street to essential users only and it may be that the proposals for St Johns Street are progressed along with the adjacent Southern Gateway development area.	5,258	£336.44	£1,769,000
	11	Birmingham Road/Greenhill Junction/St Johns Street Junction*		Containing two junctions that form major gateways into the city centre and separating the station from the new Southern Gateway site and onto into the city, addressing the highway infrastructure and traffic-dominance of this corridor is major consideration. Space would be found for the pedestrian, cyclist and green infrastructure along a new boulevard. This project could conceivably be developed alongside the Southern Gateway to ensure a holistic, cohesive design.	15,675	£581.24	£8,659,000
Non-Scheduled			Comments	Project Rationale	Area m2	All in Rate £	Order of Cost £
Projects	8	Minster Pool Walk and Gardens		The public realm improvements within this area would be realised in conjunction with the completion of the Bird Street Car Park major development area.	4,688	£299.49	£1,181,000
		Floating Stage Provisional Sum					£540,000
Total (excluding VAT and other listed items)							£26,391,000

Success Criteria

Key

-  Little / No Impact
-  Low Impact
-  Medium Impact
-  High Impact

Projects

1. Market Square /Breadmarket Street /Bore Street /Conduit Street/ Tamworth Street
2. Bore Street (East)
3. Market Street
4. Bird Street Gateway
5. Bird Street/Sandford Street/ Car Park entrance
6. Dam Street
7. Lloyd's Walk
8. Minster Pool Walk and Gardens
9. Reeve Lane
10. Wade Street/Castle Dyke/Frog Lane
11. Birmingham Road/Greenhill Junction/St Johns Street Junction
12. St Johns Street
13. Station Square
14. Tamworth Street/Lombard St/ George Ln (parts)
15. The Friary

Measures of Success

	Hotel bed nights	City centre vacancy numbers	Footfall	Retail Spend/turnover/sales	Nighttime economy	Gross Value Added (GVA)	Retail diversity	Place attractiveness	Events and Festival days	Resident numbers within city centre	Customer/visitor/resident satisfaction	Ease of movement	Cycle and walking usage	Air Quality	Green Infrastructure (tree cover)
1. Market Square /Breadmarket Street /Bore Street /Conduit Street/ Tamworth Street	Medium	Medium	Medium	Medium	Low	High	Medium	High	High	Low	High	Medium	Low	None	None
2. Bore Street (East)	Medium	Medium	Low	Medium	Medium	Medium	Low	High	High	Low	Medium	Low	Low	None	None
3. Market Street	Medium	High	High	High		High	Medium	High	Medium	Low	High	Medium	Low	None	None
4. Bird Street Gateway	Low	None	None	None	Low	Low	None	Low	Low	Low	Low	High	Medium	Medium	None
5. Bird Street/Sandford Street/ Car Park entrance	High	High	High	None	High	High	Low	High	High	Medium	High	Medium	Low	Low	None
6. Dam Street	Medium	Medium	High	Medium	Low	Medium	Medium	Medium	High	Low	High	Low	Low	None	None
7. Lloyd's Walk	None	None	High	Low	Medium	Medium	None	Medium	None	None	High	High	Medium	Low	Medium
8. Minster Pool Walk and Gardens	High	Low	Low	Low	Medium	Medium	None	High	High	Medium	High	Medium	Medium	None	Medium
9. Reeve Lane	None	None	None	None	None	Low	None	Low	Medium	None	Low	Medium	Medium	None	Low
10. Wade Street/Castle Dyke/Frog Lane	None	None	Low	None	Low	Low	None	Medium	Medium	Low	Medium	Low	Low	Low	Medium
11. Birmingham Road/Greenhill Junction/St Johns Street Junction	None	None	Low	None	None	Low	None	High	None	None	Medium	High	High	High	High
12. St Johns Street	Low	Medium	Low	Low	Low	Medium	Medium	High	Low	Low	Medium	Medium	Medium	High	Low
13. Station Square	Medium	Low	Medium	Low	High	High	None	High	Medium	Medium	High	High	High	High	Low
14. Tamworth Street/Lombard St/ George Ln (parts)	None	Low	Medium	Low	Low	Medium	Medium	Medium	Low	Low	Low	Medium	Medium	Medium	Low
15. The Friary	Low	Low	Medium	Low	Medium	Medium	None	High	Medium	Low	Medium	High	High	Medium	High

ORDER OF COST SUMMARY - Basis of Pricing and List of Exclusions**A Basis of Pricing and Next Action**

- 1.1 The order of cost is based on Austin-Smith Lord Public Realm Strategy Report - dated September 2021 and indicated projects 1 to 16
- 1.2 Pricing is based on present value as 3Q 2021.
- 1.3 The order of costs are based on approximate areas for each project, as indicated in the Public Realm Report. The order of cost rates have been bench marked against the quality of materials within the Report and surface material palettes (Type A to D as indicated).
- 1.4 The all in rates for each project (as indicated in the cost summary) include allowances for new surface finishes to footpaths and roads, furniture including seating, bins, bollards and cycle racks, unless indicated otherwise and Landscaping. The allowances for surface finishes, furniture and landscaping for each project is included in the elemental summary.
- 1.5 The following allowance have been included - 10% Contingency, 6% OH & P, 15% Preliminaries, 15% Fees (any exclusions are indicated in the list below)
- 1.6 The 15% Fees shall include Landscape Architect, MEP, Structure and Civil Engineer, Highway Engineer, Conservation Architect.
- 1.7 City wide Lighting Strategy - £80k Fees to develop a brief for pricing
- 1.8 City wide Public Art Strategy - £25k Fees to develop a brief for pricing
- 1.9 City Wide Signage and Wayfinding Strategy - £30k Fees to develop a brief for pricing
- 1.10 Floating Stage in the Minster Pool - A provisional sum of £540,000 has been included subject to design input. This is assumed would form part of project 8 scope of work.
- 1.11 Each project has been allocated a recommendation of priority of implementation from category 1 to 3. The reason for each category is indicated in the Austin-Smith Lord Report in Figure 6 on page 71. Note category 1 is recommended/ seen as the first priority to be implemented
- 1.12 The next recommended action is a Public Realm programme of implementations is developed and co-ordinated with Lichfield City Master Plan and agreed with all the Lichfield City stakeholders. This includes the separate development sites such as: Bird Street Car Park site, Birmingham Road site/ District Council House and the University West Car Park site. The Citywide public realm projects design needs developing to give an overall consistent City approach of implementation of external lighting, information/wayfinding signage and public art work etc.
- 1.13 Basis of the Cost Estimate

The cost estimate for the Surface Finishes category shall include the following allowance:

- Removing existing surface finishes and replace with proposed finishes and kerbs as tabulated in the Surface Material Palettes for all the roads and areas as shown on the drawing.

- Provision of drainage connection for every 50 meters of the new kerbs.
- Extra over top soil removal and earthwork for the Station Forecourt
- Extra over new crossroad, cycle way and station court pavement at the Birmingham Road.
- Extra over for new round about and cycle way in additional to the resurfacing allowance at the Friary.

The cost estimate for the Street Furniture category shall include the following allowance:

- Removal of existing uncoordinated street furniture.
- In total of 50nr @ £3000 per Tree pit covers – 20 nrs to The Friary and 30 Nr to Birmingham Road.

ORDER OF COST SUMMARY - Basis of Pricing and List of Exclusions

- In total of 170nr @ £300 per bollard for all the streets.
- In total of 150nr @ £650 per cycle rack for all the streets.
- Provisional sum of £250k cycle hub at the Station Forecourt.
- In total of 60nr @ £500 per bin for all the streets
- In total of 80nr @ £3000 per seat, imported Falco timber Zitbank or similar for all the streets

- In total of 25nr @ £3000 per pop up power at the Market Square
- In total of 5nr @ £5000 per water point at the Market Square

The cost estimate for the Landscaping category shall include the following allowance:

- Maintenance of the existing green areas along the streets as shown on the google map with the budget range from £10 per sqm to £60 per sqm.
- Extra over for the large semi mature trees and rain garden with hard landscape rooting zone are allowed at the Friary (200m length rooting zone and 20 nrs big trees)
- Extra over for the large semi mature trees and rain garden with hard landscape rooting zone are allowed at the Birmingham Road (400m long rooting zone and 30 nrs big trees)
- Extra over for £2500 large semi mature tree 25cm to 30cm girth

B Exclusions**1 Financial, Legal & Statutory Related Exclusions**

- 1.1 VAT
- 1.2 Land acquisition costs.
- 1.3 Finance costs during construction.
- 1.4 Funding costs.
- 1.5 Legal fees
- 1.6 Specialist i.e. Archaeologist, Ecologist, Historical research, Artist etc
- 1.7 Breeam Fees
- 1.8 Party Wall Act 1996, covenant, rights of light, rights of way, etc.
- 1.9 Section agreements generally to include s.106, s.278, s215 and CIL etc
- 1.10 Planning & Building Control fees and charges.
- 1.11 Future inflation fixed price increases after 3Q 2021
- 1.12 Exchange rate/market fluctuations following the UK leaving the EU.
- 1.13 Allowances for LDC potential changes, design development and the like
- 1.14 Potential risks/ additional costs caused as a consequence of the Coronavirus (COVID-19) outbreak. It is therefore, recommended that the client makes sufficient budgetary allowances for such risks in their investment or development appraisal for the project.
- 1.15 Third party agreements and associated costs associated with land owned by Three Spires or the Cathedral, if applicable

2 Site Related Exclusions

- 2.1 Contamination and asbestos surveys and potential consequential removal or alternative measures generally
- 2.2 Abnormal ground conditions.
- 2.3 Himalayan Balsam, Japanese Knotweed or Maretail surveys and potential consequential removal/treatment
- 2.4 Capacity of the incoming mains services infrastructure
- 2.5 Attenuations ponds and/ or attenuation measures generally
- 2.6 Archaeological survey and potential works
- 2.7 Environmental survey and protection

ORDER OF COST SUMMARY - Basis of Pricing and List of Exclusions

2.8 Ground stabilisation and remediation

3 Construction Related Exclusions

- 3.1 Works to Cathedral Quarter - The Report considers the existing materials within in this area is of high quality and appropriate.
- 3.2 Works to other City Developments sites including: Bird Street Car Park site, Birmingham Road site/ District Council and University West Car Park site
- 3.3 Citywide lighting up grade but excluded feature lighting for the key building (£80k fees included)
- 3.4 Citywide public artwork works (new and existing restoration of existing feature, status, architecture)(£25k fees included)
- 3.5 Birmingham Road high way works and junction potential alteration works adjacent to the railway station/railway bridge etc (except the Friary and Birmingham roads)
- 3.6 Green roofs and walls - It is assumed will form part of the other City Development sites.
- 3.7 Citywide car park works
- 3.8 Citywide electric charging points (allowance only at the Market Square)
- 3.9 Diversion / termination of any existing services crossing / adjacent the site
- 3.10 Incoming utilities and associated BWIC
- 3.11 Upgrading the existing drainage (included allowance for drainage connection)
- 3.12 increase of existing drainage capacity excluded
- 3.13 Citywide information and wayfinding signage works (£30k fees included)
- 3.14 Works to existing planters, parapet walls and railing next to or attached the existing buildings
- 3.15 Restoration of the existing historical figure i.e. Heart of England Way pavement markers

BMMS0318
Dated: 06/10/2021**Footpaths and Roads - Surface Material Palettes****Market Quarter****Type A****Footpaths :**

Yorkstone paving (riven); 500-700mm coursing random length

Kerbs:

Yorkstone; 180mm wide x 125mm high x random length

Channels:

River cobbles set in mortar with black granite cube edgings

Carriageway:**Shared surface:**

Yorkstone sett , random coursed

Type B**Reeve Lane - Only****Footpaths :**

Yorkstone paving (riven); 600mm coursing random length (laid to north only)

Kerbs:

Yorkstone - flush; 250mm wide x 125mm high x random lengths

Channels:

Dark grey/ black granite 250mm wide x random lengths

Carriageway:

Fibredec or similar

Shared surface:

Yorkstone sett , random coursed

Southern Gateway Quarter**Type C****Footpaths :**

Yorkstone paving (diamond sawn); 500mm coursing x random length

Kerbs:

Yorkstone; 180mm wide x 125mm high x random lengths

Channels:

Concrete

Carriageway:

Bitmac/ concrete block

Shared surface:

Floating Stage Provisional Sum

Tumbled concrete setts, multi, random coursed

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Dated: 06/10/2021

Lichfield District Council
Public Realm Strategy



Footpaths and Roads - Material Palettes

Business & Learning Quarter

Type D

Footways :

Yorkstone paving (frame); 500-700mm coursing x random length

Infill paving/ trims :

Trumbled, silver grey concrete setts 150 x 150mm and tumbled concrete setts, red multi random coursed

Kerbs:

Conservation kerb, silver grey

Channels:

Conservation channel

Carriageway:

Bitmac

Shared surface:

Tumbled concrete setts, multi, random coursed

BMMS0318
Dated: 06/10/2021

Lichfield District Council
Public Realm Strategy



ORDER OF COST ELEMENTAL SUMMARY

Projects	Public Realm Areas/ Scope of Works	Total £	Elements of Work Cost				Sundry Add On Cost Items					
			A1. Surface Finishes	A2. Street Furniture	A3. Landscaping	Sub Total	Contingency	OH & P	Prelim	Fees	Sub Total	
							10%	6%	15%	15%		
Section A	<u>Surface Finish, Furniture and Landscaping</u>											
	<i>Market Quarter (surface material palette "A")</i>											
Project 1	Market Square/ Breadmarket Street/ Bore Street/ Conduit Street/ Tamworth Street	£217,890.78	£2,700.00	£124,800.00	£13,800.80	£141,300.80	£14,130.08	£9,325.85	£24,713.51	£28,420.54	£76,589.98	
Project 2	Bore Street East	£194,446.76	£105,800.00	£19,700.00	£597.50	£126,097.50	£12,609.75	£8,322.44	£22,054.45	£25,362.62	£68,349.26	
Project 3	Market St	£439,910.05	£263,784.00	£20,000.00	£1,494.90	£285,278.90	£28,527.89	£18,828.41	£49,895.28	£57,379.57	£154,631.15	
Project 4	Bird Street Gateway	£1,216,974.02	£699,024.00	£33,395.00	£56,781.00	£789,200.00	£78,920.00	£52,087.20	£138,031.08	£158,735.74	£427,774.02	
Project 5	Bird Street/ Sandford Street/ Car Park entrance	£1,141,260.26	£707,996.00	£28,392.00	£3,712.10	£740,100.10	£74,010.01	£48,846.61	£129,443.51	£148,860.03	£401,160.16	
Project 6	Dam Street	£561,578.30	£313,408.00	£26,115.00	£24,657.00	£364,180.00	£36,418.00	£24,035.88	£63,695.08	£73,249.34	£197,398.30	
Project 7	Lloyd's Walk	£71,686.12	£26,152.00	£19,650.00	£686.00	£46,488.00	£4,648.80	£3,068.21	£8,130.75	£9,350.36	£25,198.12	
Project 8	Minster Pool Walk and Gardens	£1,181,096.43	£662,016.00	£20,937.00	£82,980.60	£765,933.60	£76,593.36	£50,551.62	£133,961.79	£154,056.06	£415,162.83	
	Floating Stage (provisional sum)	£539,712.25	£350,000.00			£350,000.00	£35,000.00	£23,100.00	£61,215.00	£70,397.25	£189,712.25	
	<i>Reeve Lane (surface material palette "B")</i>											
Project 9	Reeve Lane	£254,886.51	£130,528.80	£26,080.00	£8,683.50	£165,292.30	£16,529.23	£10,909.29	£28,909.62	£33,246.07	£89,594.21	
	<i>Southern Gateway Quarter (surface material palette "C")</i>											
Project 10	Wade Street/Castle Dyke/Frog St/ Southern Gateway	£1,988,605.86	£1,258,264.00	£24,728.00	£6,606.40	£1,289,598.40	£128,959.84	£85,113.49	£225,550.76	£259,383.37	£699,007.46	
Project 11	Birmingham Road/Greenhill Junction/St Johns Street Junction	£8,658,693.07	£3,698,640.00	£132,778.00	£1,783,690.00	£5,615,108.00	£561,510.80	£370,597.13	£982,082.39	£1,129,394.75	£3,043,585.07	
Project 12	St Johns Street	£1,769,334.04	£970,788.00	£18,880.00	£157,734.00	£1,147,402.00	£114,740.20	£75,728.53	£200,680.61	£230,782.70	£621,932.04	
Project 13	Station Square	£2,171,317.30	£1,100,049.00	£299,845.00	£8,191.62	£1,408,085.62	£140,808.56	£92,933.65	£246,274.17	£283,215.30	£763,231.68	
Project 14	Tamworth St/ Greenhill	£719,376.60	£442,472.00	£21,720.00	£2,319.20	£466,511.20	£46,651.12	£30,789.74	£81,592.81	£93,831.73	£252,865.40	
	<i>Business & Learning Quarter (surface material palette "D")</i>											
Project 15	The Friary	£5,129,372.80	£2,252,308.00	£97,155.00	£976,903.00	£3,326,366.00	£332,636.60	£219,540.16	£581,781.41	£669,048.63	£1,803,006.80	
Section B	Citywide Lighting Strategy - Cost to Develop a Strategy only	£80,000.00								£80,000.00	£80,000.00	
Section C	Citywide Signage/ Wayfinding Strategy - Cost to Develop Strategy only	£30,000.00								£30,000.00	£30,000.00	
Section D	Citywide Public Art Strategy - Cost to Develop a Strategy only	£25,000.00								£25,000.00	£25,000.00	
		£26,391,141.15	£12,983,929.80	£914,175.00	£3,128,837.62	£17,026,942.42	£1,702,694.24	£1,123,778.21	£2,978,012.22	£3,559,714.06	£9,364,198.73	

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Feasibility Report

Lichfield City Centre Pedestrianisation

D4281K / 001 Revision 01

04/08/2021

ameyconsulting



Document Control Sheet

Project Name:	Lichfield City Centre Pedestrianisation
Project Number:	D4281K
Report Title:	Feasibility Report
Report Number:	001

Issue Status/Amendment	Prepared	Reviewed	Approved
001	Name: Tristram Lee-Jones Signature: Date:	Name: Tony Cahill Signature: Date:	Name: Sandeep Aheer Signature: Date:
	Name: Signature: Date:	Name: Signature: Date:	Name: Signature: Date:
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	Name: Signature: Date:	Name: Signature: Date:	Name: Signature: Date:

1. Executive Summary

This report outlines options to reduce vehicle numbers within Lichfield City Centre pedestrian zone, to redress the balance between pedestrians and motor vehicles. The options have been designed to improve pedestrian safety, the public realm environment and to promote sustainable travel within the City Centre.

Contents

2. Introduction.....	4
2.1. Scope	4
2.2. Site Description	4
3. Existing Site Conditions.....	5
3.1. Existing Traffic Regulation Orders.....	5
3.1.1 Pedestrian Zones.....	5
3.1.2 One-Way Restrictions	7
3.1.3 Parking Restrictions	7
3.2. Existing City Centre Parking Provision	9
3.3. Traffic and Pedestrian Surveys	11
3.4. Accident Data	22
4. Site Observations	23
4.1. Pedestrian Zones	23
5. Proposals	24
5.1. Access Enforcement Options.....	24
5.2. Traffic Regulation Order Access Restriction Proposals.....	26
5.2.1 Option 1 (Drg No. D4281K.R01.001).....	26
5.2.2 Option 2 (Drg No. D4281K.R01.002).....	27
5.2.3 Option 3 (Drg No. D4281K.R01.003).....	28
5.2.4 Bird Street One Way System Alteration Options (Drg No. D4281K.R01.004).....	29
5.3. Pedestrian Improvements.....	30
6. Street Furniture	31
7. Recommendations / Summary.....	32

Tables

Table 1: Recommended maximum walking distance without a rest according to disability (LTN 5-95)	9
Table 2: Average daily vehicle counts Lichfield City Centre.....	21

Figures

Figure 1: Lichfield City Centre pedestrianisation scheme extents.....	4
Figure 2: Pedestrian zone traffic regulation orders	6
Figure 3: One-way restrictions	7
Figure 4: Tamworth Street parking restriction.....	8
Figure 5: Conduit St, Market St (part), Breadmarket St & Bore St (part) parking restrictions.....	8
Figure 6: Bore Street (From Breadmarket Street to St Johns Street) parking restrictions	9
Figure 7: Lichfield City Centre parking provisions	10
Figure 8: Tamworth Street junction with Conduit Street, market day (Tues, Fri, Sat) daily average vehicle and pedestrian movements (7am-7pm).....	12
Figure 9: Tamworth Street junction with Conduit Street, non-market day (Mon, Wed, Thurs) daily average vehicle and pedestrian movements (7am-7pm).....	12
Figure 10: Conduit Street junction with Market Street and Dam Street, market day (Tues, Friday, Saturday) daily average vehicle and pedestrian flows (7am-7pm).....	13
Figure 11: Conduit Street junction with Market Street and Dam Street, non-market day (Mon, Wed, Thurs) daily average vehicle and pedestrian movements (7am-7pm)	14
Figure 12: Market Street junction with Breadmarket Street, market day (Tues, Friday, Saturday) daily average vehicle and pedestrian flows (7am-7pm)	15
Figure 13: Market Street junction with Breadmarket Street, non-market day (Mon, Wed, Thurs) daily average vehicle and pedestrian movements (7am-7pm).....	15
Figure 14: Breadmarket Street junction with Bore Street, market day (Tues, Friday, Saturday) daily average vehicle and pedestrian flows (7am-7pm).....	16
Figure 15: Breadmarket Street junction with Bore Street, non-market day (Mon, Wed, Thurs) daily average vehicle and pedestrian movements (7am-7pm).....	17
Figure 16: Bore St junction with St John St and Bird St junction with The Friary, market day (Tues, Friday, Saturday) daily average vehicle and pedestrian flows (7am – 7pm).....	18
Figure 17: Bore St junction with St John St and Bird St junction with The Friary, non-market day (Mon, Wed, Thurs) daily average vehicle and pedestrian flows (7am – 7pm).....	18
Figure 18: Vehicle observed making an illegal right turn out of Bore Street onto The Friary	19
Figure 19: Vehicle observed making an illegal right turn out of Bore Street across a controlled pedestrian crossing and onto The Friary	19
Figure 20: Vehicle observed illegally driving southbound on Bird Street before turning left onto St John Street)	20

Appendices

Appendix A: Drawings	34
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2. Introduction

2.1. Scope

This report has been produced by the Design Team following a brief received from Strategic Transport and Community Highways working together under the terms of the Staffordshire County Council Infrastructure + partnership contract.

The commission considers the possible options in relation to the existing pedestrianised Lichfield City Centre in order to redress the balance of vehicles and pedestrians and to review the implications of each design option.

2.2. Site Description

Lichfield City is an historic city located in Staffordshire, 18 miles north of Birmingham. Lichfield (Figure 1) is famed for its three spired Cathedral and vibrant city centre, hosting outdoor markets three times a week (Tuesday, Friday and Saturday) in its Market Square. The streets are lined with historically significant buildings, accommodating a range of boutique stores, eating establishments and other businesses.

The streets making up the city centre are largely in a pedestrian zone, with access to Tamworth Street, Conduit Street, Market Street, Breadmarket Street and Bore Street for permit holders, disabled badge holders and for loading. Bird Street and Sandford Street are subject to similar restrictions except disabled badge holders are not permitted at any time. The nature of the streets mean pedestrians are often unaware of the presence of vehicles causing concerns for safety, particularly for more vulnerable pedestrians.

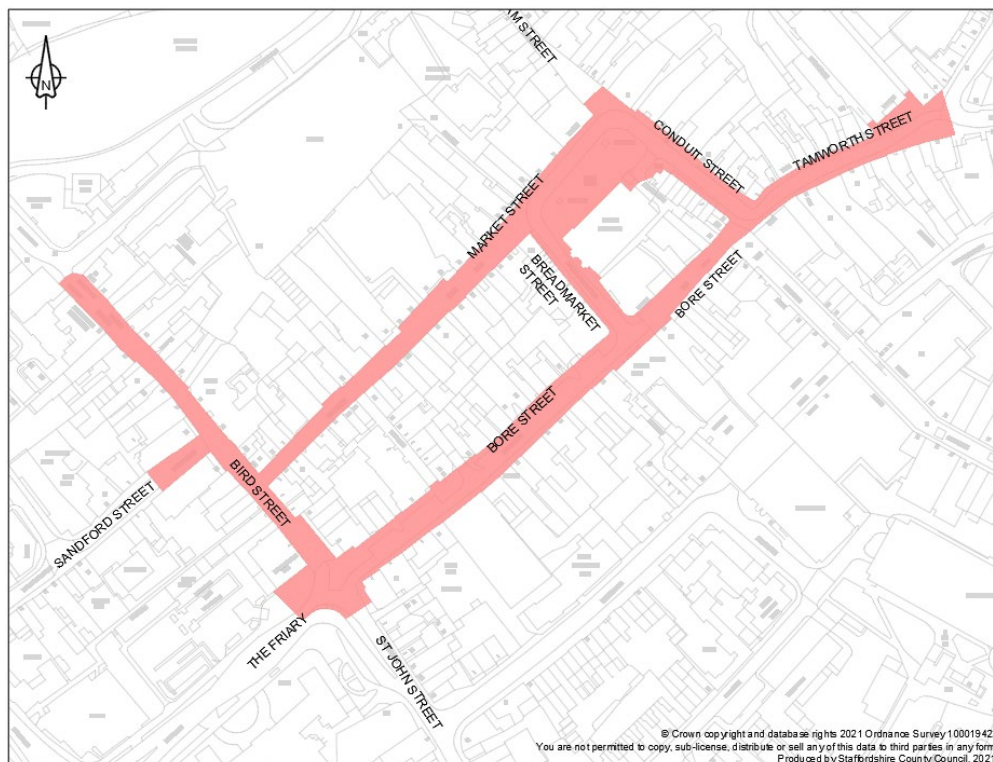


Figure 1: Lichfield City Centre pedestrianisation scheme extents

3. Existing Site Conditions

3.1. Existing Traffic Regulation Orders

3.1.1 Pedestrian Zones

Lichfield City Centre is subject to two different pedestrian zone traffic regulation orders (Figure 2). All vehicles except for loading, permit holders, disabled and cycles are prohibited in Tamworth Street, Bore Street (part of), Conduit Street, Breadmarket Street and Market Street (part of), between the hours of 9am and 6pm.

Dam Street, Bird Street and the remaining lengths of Market Street and Bore Street are restricted to all motor vehicles at any times. Similar to the preceding restrictions, loading, cyclists and permit holders are permitted to enter the zone, however, disabled badge holders are not. It should be noted that this section of Bore Street is not signed on site and disabled users would be unaware they are not permitted to drive this section, however, the road alignment at the junction of Tamworth Street and Conduit Street discourages this manoeuvre.

Loading is permitted within both zones by any vehicle except on Friday and Saturday 10am – 4pm. 'Loading generally refers to commercial loading or to objects that are too heavy or bulky to be carried very far by hand but does not include time for purchasing the goods' chapter 3 Traffic signs manual. The Pedestrian zone restrictions within Lichfield permit loading by any vehicle not just by goods vehicle.

The wording within the Traffic regulation permits access for Taxis requiring access to residential properties to pick up or drop off passengers. Taxis are also permitted to access Bird Street via Sandford Street between 6pm and 9am. The definition of a Taxi given within the traffic regulation order includes both Hackney Carriages and licenced Private Hire vehicles.

Permits to enter the pedestrian zone are issued by Staffordshire County Council. Those eligible for a permit as stipulated in the associated Traffic Regulation Order are:

- A person or business that has unavoidable operational or personal difficulties
- A business which has exceptional servicing problems due to the nature of the operations
- Business premises within the central area which have off-street parking spaces
- Residents living within the central area who have off-street car parking space(s)
- Residents living within the central area who do not have an off-street parking space



Figure 2: Pedestrian zone traffic regulation orders

3.1.2 One-Way Restrictions

The streets within the Pedestrian zone form a one-way system (Figure 3) with access permissible from Sandford Street, The Friary and Tamworth Street.

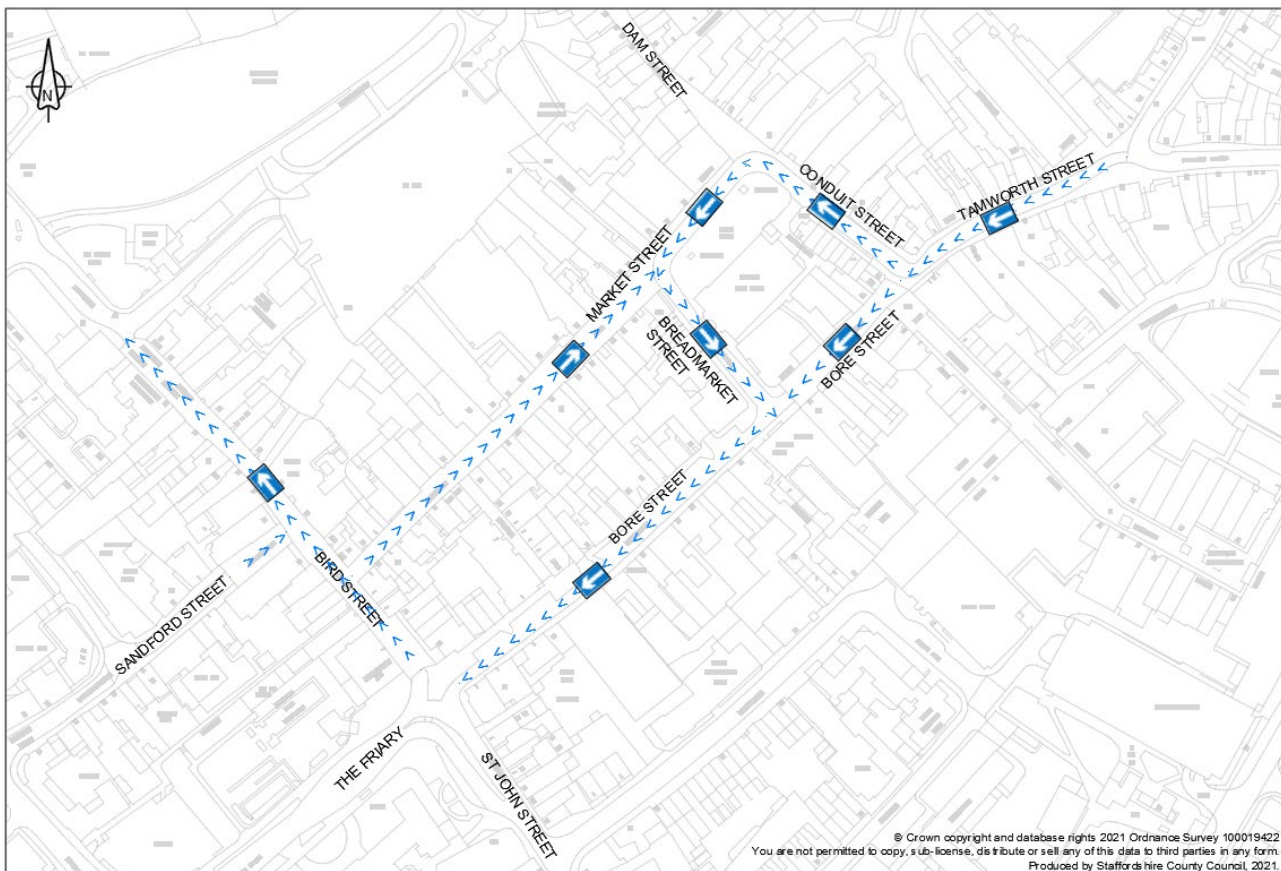


Figure 3: One-way restrictions

3.1.3 Parking Restrictions

A traffic regulation order restricts all streets within the pedestrian zones to no waiting at any time, except in signed bays. This is indicated on site using signs at the zone entry points removing the need for double yellow line road markings. It should be noted that the text "except in signed bays" is not present on the existing signage which may cause ambiguity.

All parking within signed bays is restricted to disabled badge holders only between 9am and 6pm and is located as follows:

- Tamworth Street (Figure 4) - parking for approximately 5 cars between 9am and 6pm with a 3 hour limit and no return within 1 hour; however, due to the Covid-19 pandemic, the parking bays have been partially suspended.

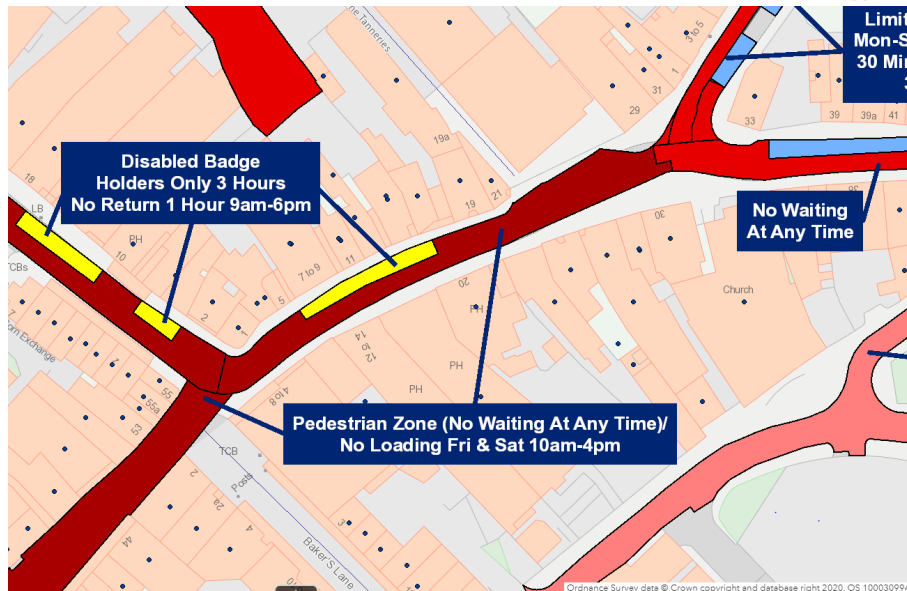


Figure 4: Tamworth Street parking restrictions

- Conduit Street (Figure 5)– parking for approximately 5 cars between 9am and 6pm, however, due to the Covid-19 pandemic, the parking bays have been partially suspended.
- Market Street (Figure 5) - parking for approximately 8 cars between 9am and 6pm, however, due to the Covid-19 pandemic, the parking bays have been partially suspended.

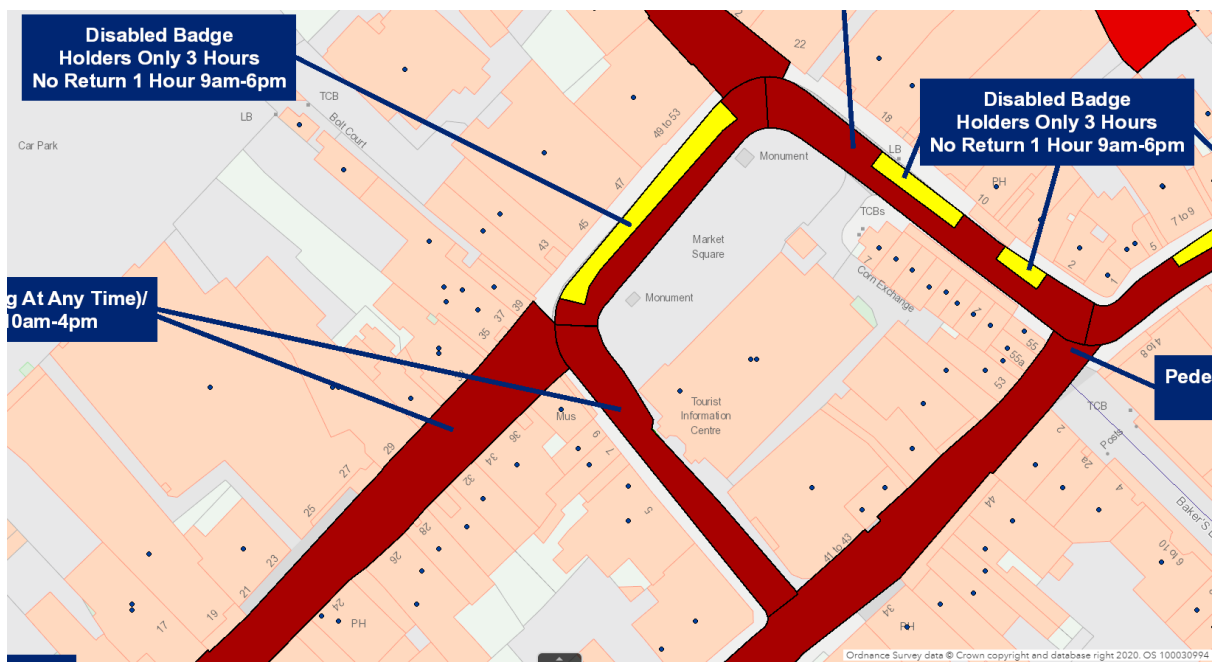


Figure 5: Conduit St, Market St (part), Breadmarket St & Bore St (part) parking restrictions

- Bore Street from Breadmarket Street to St Johns Street (Figure 6) - parking for approximately 18 cars between 9am and 6pm, however, due to the Covid-19 pandemic, the parking bays have been partially suspended.

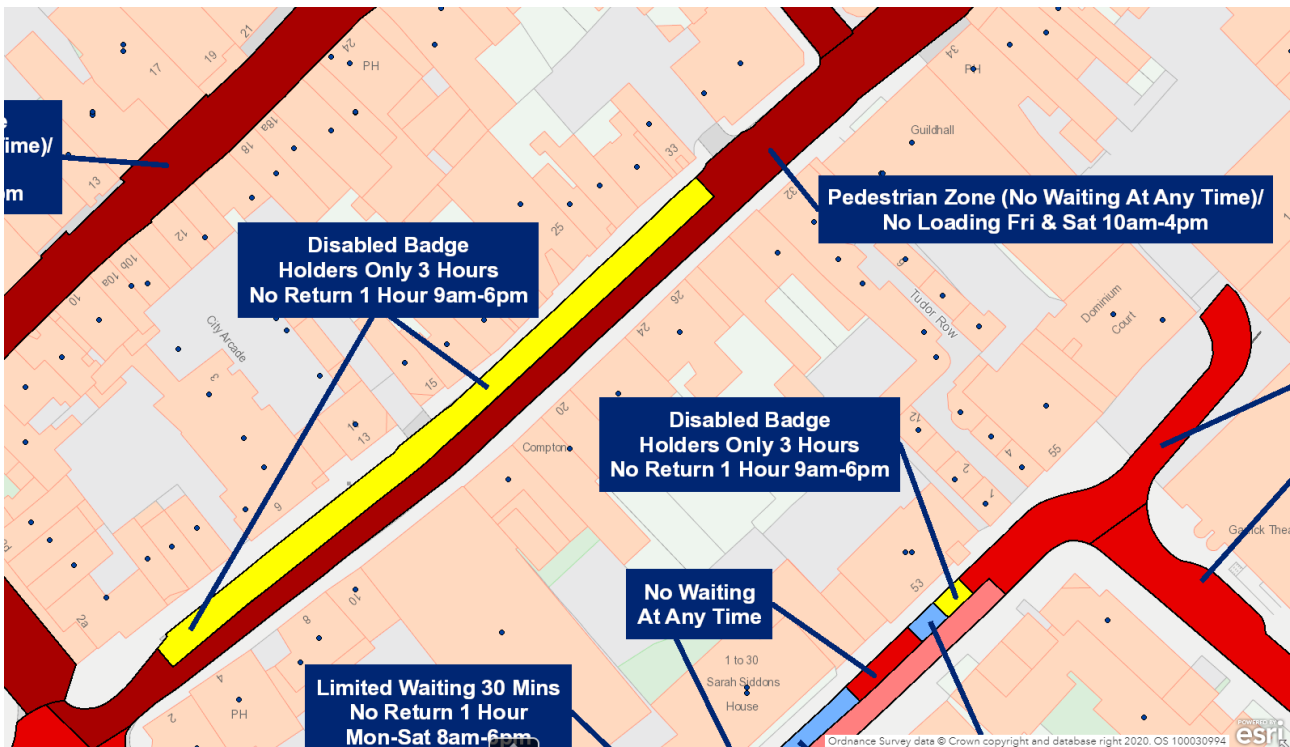


Figure 6: Bore Street (From Breadmarket Street to St Johns Street) parking restrictions

3.2. Existing City Centre Parking Provision

A significant amount of public car parking is available within the immediate vicinity of the city centre (Figure 7). Throughout the Covid-19 pandemic disabled parking bays were temporarily suspended within Tamworth Street, Conduit Street, Market Street and Bore Street to provide additional space for social distancing. To mitigate this loss, standard parking bays were reallocated to create 10 temporary disabled bays within Bird Street car park and 7 temporary disabled bays in Lombard Street car park. Guidance on the recommended maximum walking distance without rest according to disability is given in Table 1. It should be noted that this relocated disabled parking provision is approximately 65m and 125m away from the pedestrianised zone.

Table 1: Recommended maximum walking distance without a rest according to disability (TAL 5-95)

Disability	Distance (metres)
Visually Impaired	150
Wheelchair users	150
Ambulatory without walking aid	100
Stick users	50

All disabled parking within the city centre is free and there are no waiting time restrictions within the off street car parks. All on-street disabled parking is limited to 3 hours.

A minimum of 6% of spaces within a car park should be allocated for the blue badge holders (BS8300-1:2018). Existing disabled parking provision within Lichfield City centre car parks is 4% (pre Covid-19) with no provision within the University or Friary Inner car parks. When combined with all on street parking this increases to the recommended 6% due to the high number of spaces provided within the Pedestrian Zone.

Additional weekend parking provision is available at Lombard Street and District Council House (Saturday only) car parks.

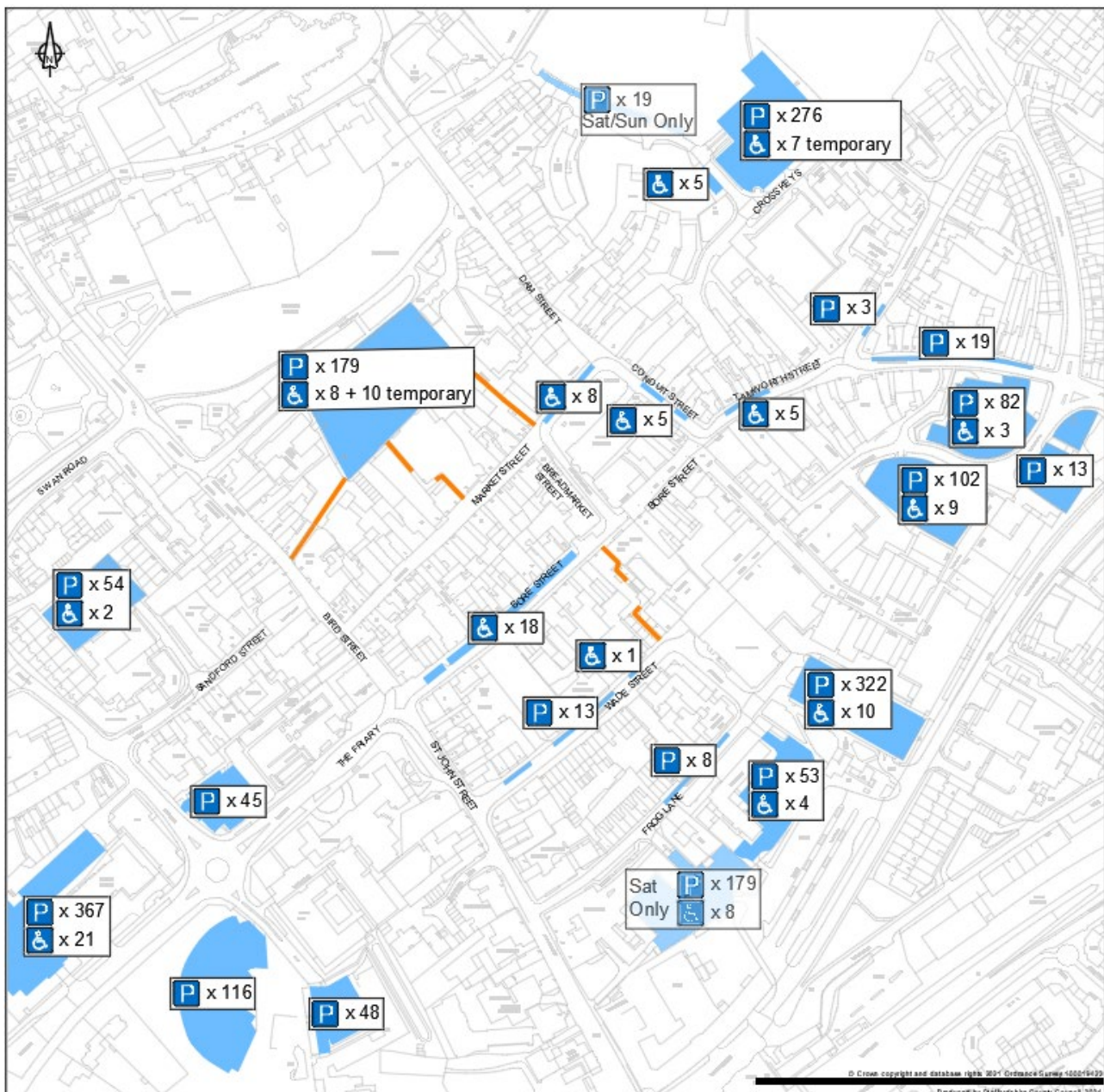


Figure 7: Lichfield City Centre parking provisions

3.3. Traffic and Pedestrian Surveys

Traffic and pedestrian surveys were undertaken on Tamworth Street, Conduit Street, Market Street, Breadmarket Street and Bore Street. The following data was collected for 7 days between 7am and 7pm from 13:00 on 21st May to 13:00 on 28th May 2021:

- 12-hour pedestrian count
- 12-hour classified junction vehicle counts

A 12-hour vehicle count survey was also undertaken at Sandford Street over the same 7 day period.

This data was collected during step 2 of the [roadmap out of lockdown](#), part of the UK government response to Covid-19 global pandemic. During step 2, shops were open, as well as hairdressers, libraries, zoo's, and gyms. Pubs, restaurants, and cafés were also open to outdoor diners only.

In response to Covid-19 and social distancing requirements, much of the on-street parking throughout Tamworth Street, Conduit Street, Market Street, Breadmarket Street and Bore Street has been temporarily suspended; as a result of this, vehicle flows are likely to be less than pre-Covid-19 levels.

Tamworth Street junction with Conduit Street

The classified junction counts undertaken at Tamworth Street junction with Conduit Street and Bore Street recorded a daily average of 546 vehicles travelling through the city centre to Conduit Street on market days (Figure 8). A slightly lower daily average vehicle count of 534 vehicles was recorded on non-market days (Figure 9). An average of 13 and 17 vehicles per day were observed to be accessing Bore Street from Tamworth Street.

On a market day, an average of 6 vehicles were observed to be making illegal manoeuvres at the junction, opposing the one-way restriction and on non-market days an average of 8 vehicles were making illegal manoeuvres.

Of the 565 vehicles driving through the junction per day on market days, around 15% are delivery vehicles (Light Goods Vehicles or larger), while on non-market days delivery vehicles account for 19% of all traffic at the junction. Over half of the total number of vehicles are using the junction between the hours of 10am and 4pm (Table 2).

Pedestrian volumes were observed to be higher on market days in all directions. The highest volume recorded a daily average of 1524 pedestrians crossing south to north on Bore Street on market days, where the vehicle flows are the lowest (Figure 8).

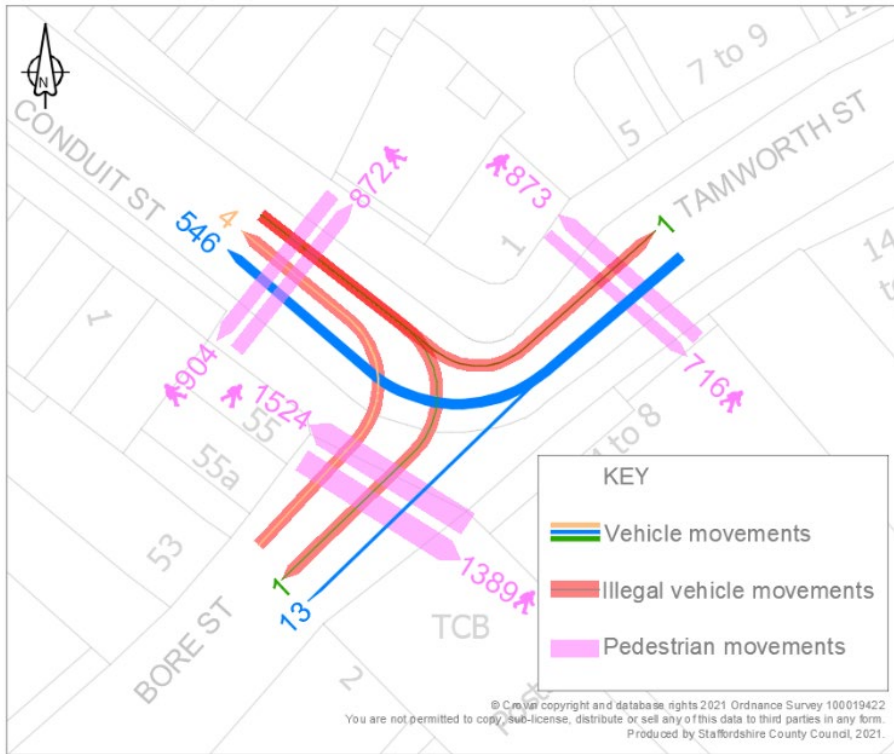


Figure 8: Tamworth Street junction with Conduit Street, market day (Tues, Fri, Sat) daily average vehicle and pedestrian movements (7am-7pm)

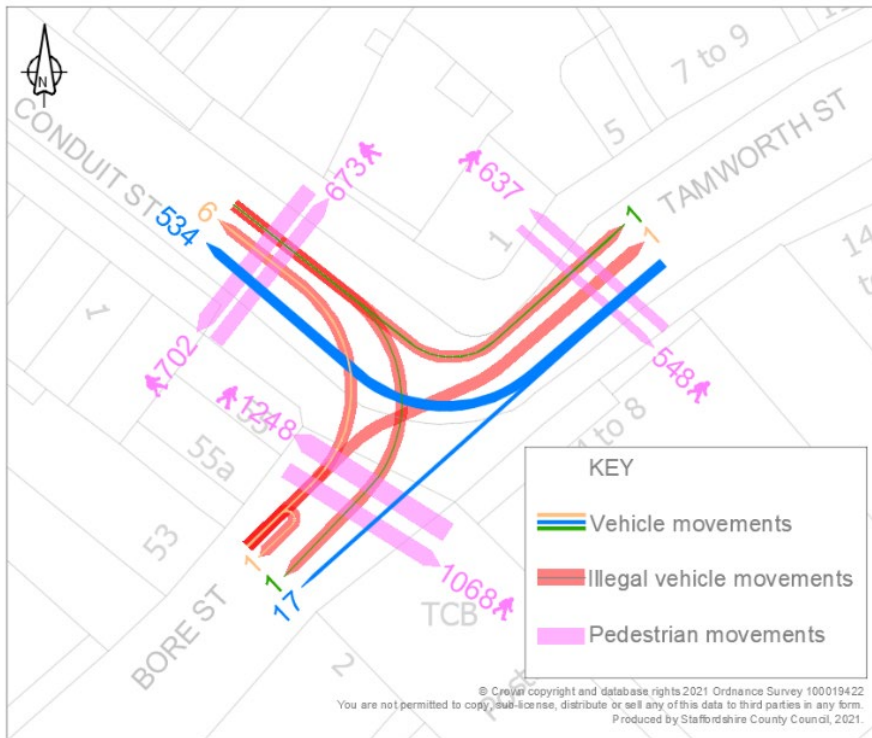


Figure 9: Tamworth Street junction with Conduit Street, non-market day (Mon, Wed, Thurs) daily average vehicle and pedestrian movements (7am-7pm)

Conduit Street junction with Market Street and Dam Street

The classified junction counts recorded at Conduit Street junction with Market Street and Dam Street recorded a daily average of 520 vehicles travelling through the city centre to Market Street on market days (Figure 10). A slightly lower daily average vehicle count of 493 vehicles was recorded on non-market days (Figure 11).

On a market day, an average of 2 vehicles were observed to be making illegal manoeuvres at the junction, opposing the one-way restriction and on non-market days an average of 3 vehicles were making illegal manoeuvres.

Of the 581 vehicles driving through the junction per day on market days, around 15% are delivery vehicles (Light Goods Vehicles or larger), while on non-market days delivery vehicles account for 19% of all traffic at the junction. Over half of the total number of vehicles are using the junction between the hours of 10am and 4pm (Table 2).

Pedestrian volumes were observed to be higher on market days crossing Conduit Street and Market Street in both directions than non-market days. Pedestrian volumes crossing Dam Street in both directions are almost double the volumes recorded crossing Market Street and Conduit Street. However, there was little difference between pedestrian volumes crossing Dam Street on market days and non-market days. The highest volume recorded a daily average of 1882 pedestrians crossing from southwest to northeast on Dam Street on market days (Figure 10).

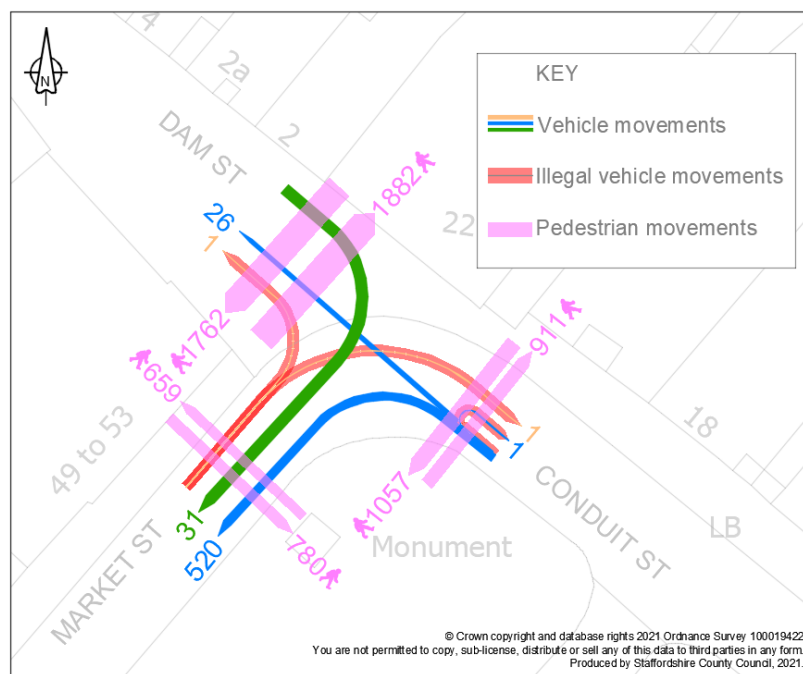


Figure 10: Conduit Street junction with Market Street and Dam Street, market day (Tues, Friday, Saturday) daily average vehicle and pedestrian flows (7am-7pm)

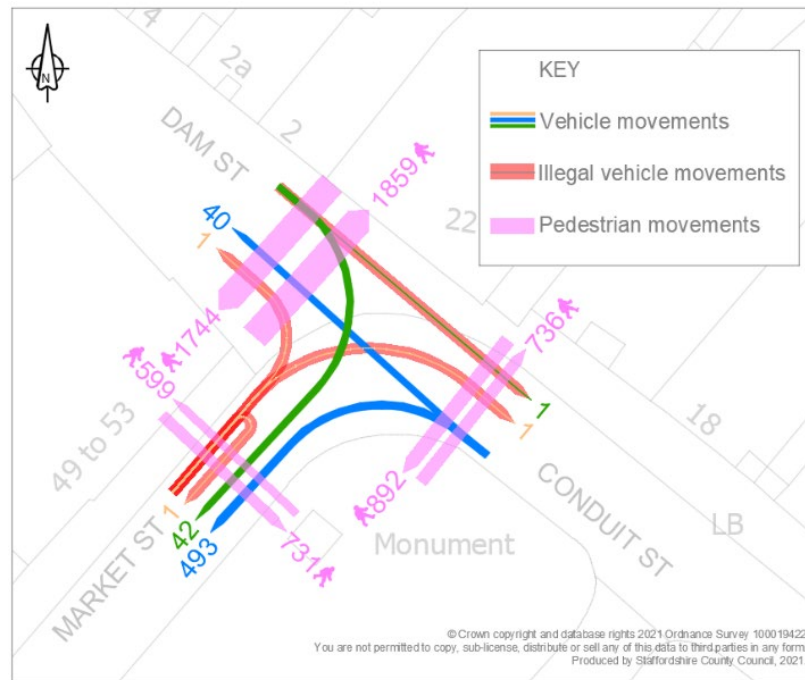


Figure 11: Conduit Street junction with Market Street and Dam Street, non-market day (Mon, Wed, Thurs) daily average vehicle and pedestrian movements (7am-7pm)

Market Street junction with Breadmarket Street

The classified junction counts recorded at Market Street junction with Breadmarket Street recorded a daily average of 555 vehicles travelling through from Market Street to Breadmarket St on market days (Figure 12). A slightly lower daily average vehicle count of 535 vehicles was recorded on non-market days (Figure 13).

On a market day, an average of 11 vehicles were observed to be making illegal manoeuvres at the junction, opposing the one-way restriction and on both market and non-market days.

Of the 620 vehicles driving through the junction per day on market days, around 17% are delivery vehicles (Light Goods Vehicles or larger), while on non-market days delivery vehicles account for 20% of all traffic at the junction. Half of all vehicles are using the junction between the hours of 10am and 4pm on market days (Table 2).

Pedestrian volumes were observed to be higher on market days for all pedestrian crossing movements. The highest volume recorded a daily average of 1610 pedestrians crossing from southwest to northeast on Breadmarket Street on market days (Figure 12).

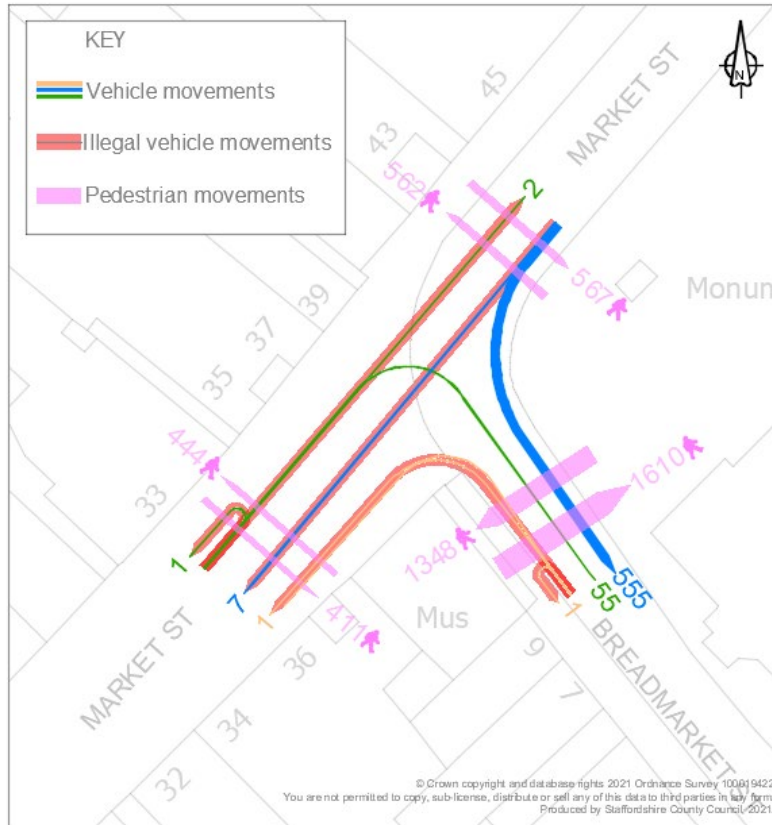


Figure 12: Market Street junction with Breadmarket Street, market day (Tues, Friday, Saturday) daily average vehicle and pedestrian flows (7am-7pm)

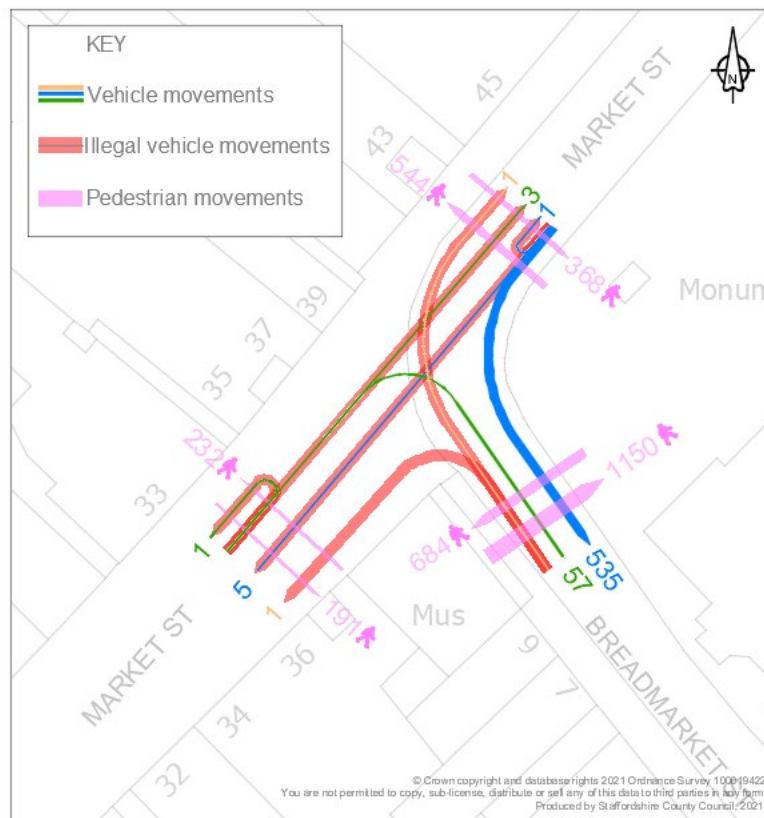


Figure 13: Market Street junction with Breadmarket Street, non-market day (Mon, Wed, Thurs) daily average vehicle and pedestrian movements (7am-7pm)

Breadmarket Street junction with Bore Street

The classified junction counts recorded at Breadmarket Street junction with Bore Street recorded a daily average of 607 vehicles travelling through from Breadmarket Street to Bore Street on market days (Figure 14). A slightly lower daily average vehicle count of 589 vehicles was recorded on non-market days (Figure 15).

On a market day, an average of 7 vehicles were observed to be making illegal manoeuvres at the junction, opposing the one-way restriction and on both market and non-market days.

Of the 630 vehicles driving through the junction per day on market days, around 17% are delivery vehicles (Light Goods Vehicles or larger), while on non-market days delivery vehicles account for 20% of all traffic at the junction. The total number vehicles travelling through the junction on non-market day is lower than market days, but the total number of vehicles between 10am - 4pm is greater on non-market days (Table 2).

Pedestrian volumes were observed to be higher on market days for all pedestrian crossing movements. The highest volume recorded a daily average of 1231 pedestrians crossing from southwest to northeast on Breadmarket Street on market days (Figure 14).

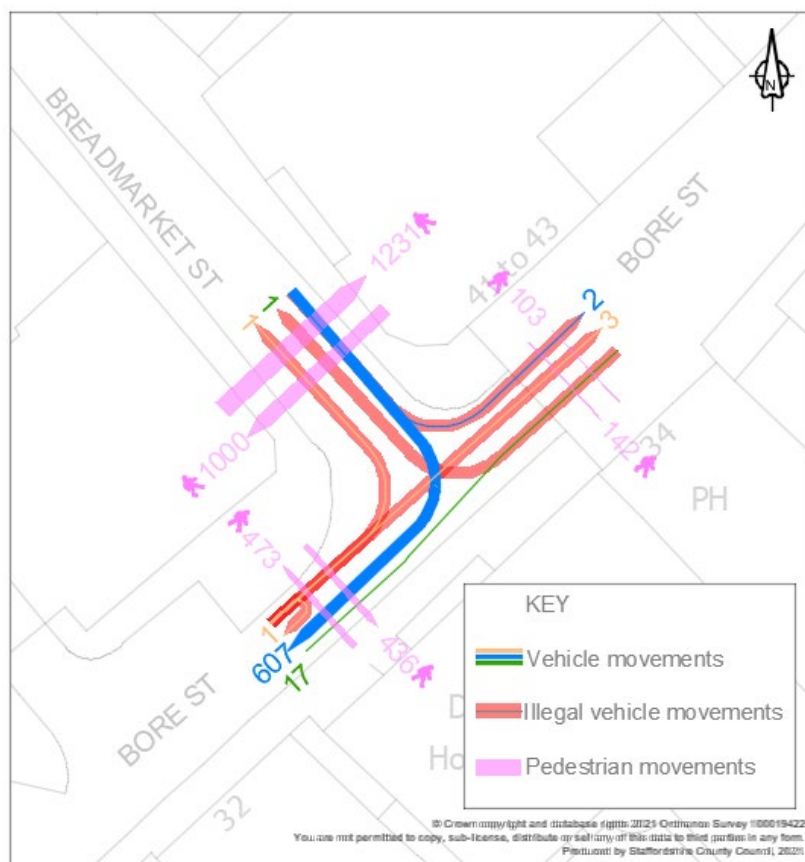


Figure 14: Breadmarket Street junction with Bore Street, market day (Tues, Friday, Saturday) daily average vehicle and pedestrian flows (7am-7pm)

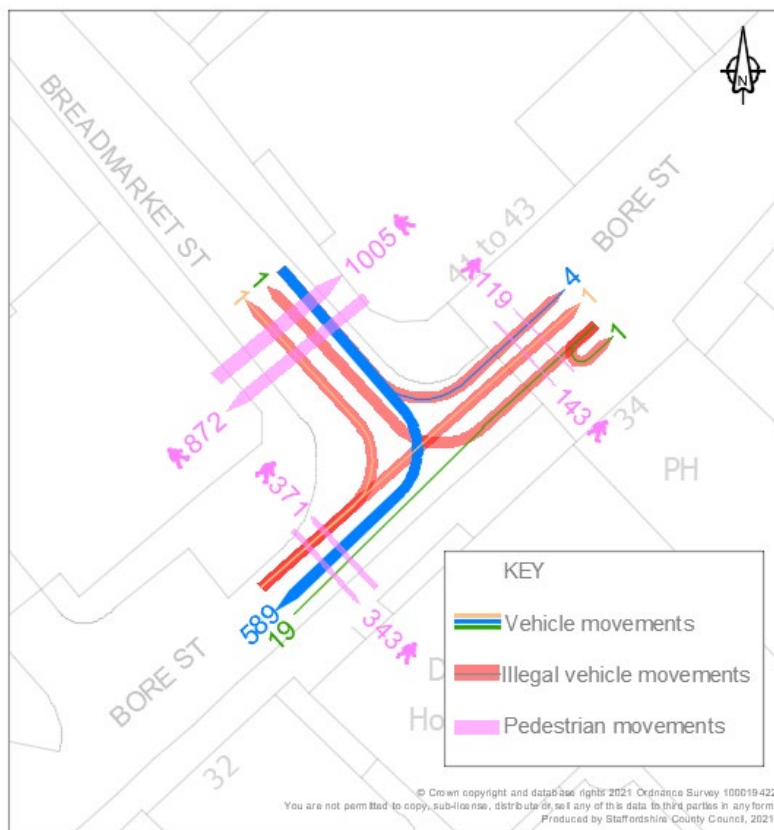


Figure 15: Breadmarket Street junction with Bore Street, non-market day (Mon, Wed, Thurs) daily average vehicle and pedestrian movements (7am-7pm)

Bore Street and Bird Street Junction with The Friary/St John Street

The classified junction counts recorded at Bore Street junction with St John Street recorded a daily average of 659 vehicles turning left from Bore Street onto St John St on market days (Figure 16) and 632 vehicles on non-market days (Figure 17). Of the total number of vehicles using the junction on market days 15% were delivery vehicles and on non-market days 17% were recorded to be delivery vehicles (Table 2). On a market day, an average of 14 vehicles were observed to be illegally turning right out of Bore Street onto The Friary, with 13 vehicles driving on the wrong side of the road to access Bird Street or The Friary and 1 vehicle driving across the pedestrian refuge island to access The Friary (Figure 18 & 19). On non-market days an average of 4 vehicles per day were illegally turning right onto the Friary (Figure 18).

On market days an average of 131 vehicles per day were accessing Bird Street, 36% of which are delivery vehicles. An average of 75 vehicles per day were recorded between 10am and 4pm with 16% of these being delivery vehicles. On non-market days 109 vehicles per day were accessing Bird Street, 30% of which are delivery vehicles. An average of 40 vehicles per day were recorded between 10am and 4pm with only 2% of these being delivery vehicles. An average of 2 vehicles per day were illegally driving southbound on Bird Street on market days and an average of 1 vehicle per day on non-market days (Figure 20).

Pedestrian volumes were observed to be higher on market days in all directions. The highest volume recorded a daily average of 1031 pedestrians crossing west to east across Bird Street junction on market days (Figure 16).

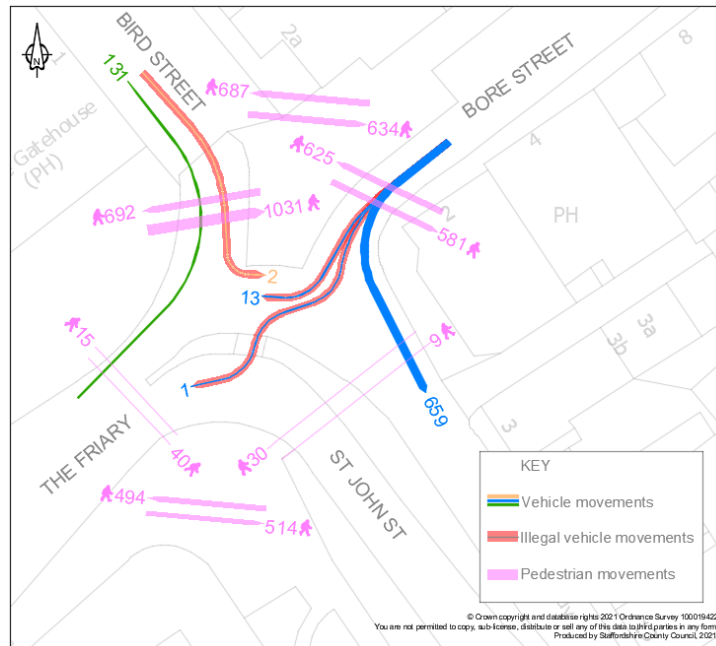


Figure 16: Bore St junction with St John St and Bird St junction with The Friary, market day (Tues, Friday, Saturday) daily average vehicle and pedestrian flows (7am – 7pm)

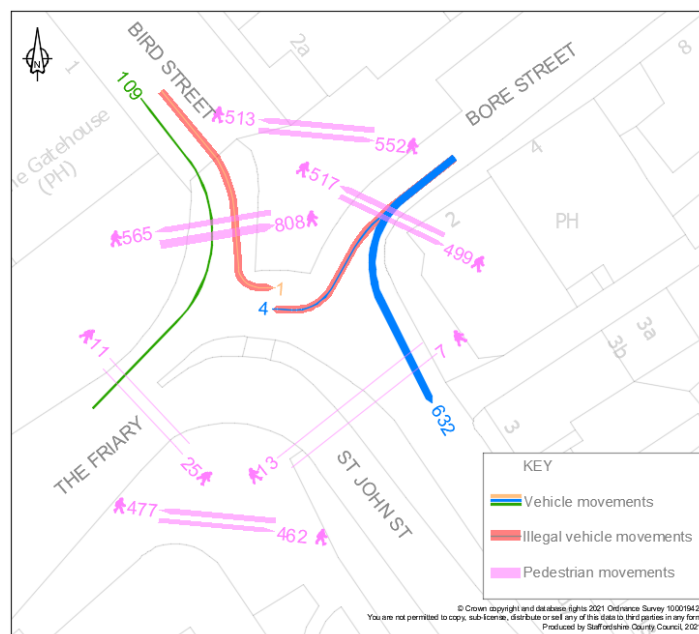


Figure 17: Bore St junction with St John St and Bird St junction with The Friary, non-market day (Mon, Wed, Thurs) daily average vehicle and pedestrian flows (7am – 7pm)



Figure 18: Vehicle observed making an illegal right turn out of Bore Street onto The Friary



Figure 19: Vehicle observed making an illegal right turn out of Bore Street across a controlled pedestrian crossing and onto The Friary

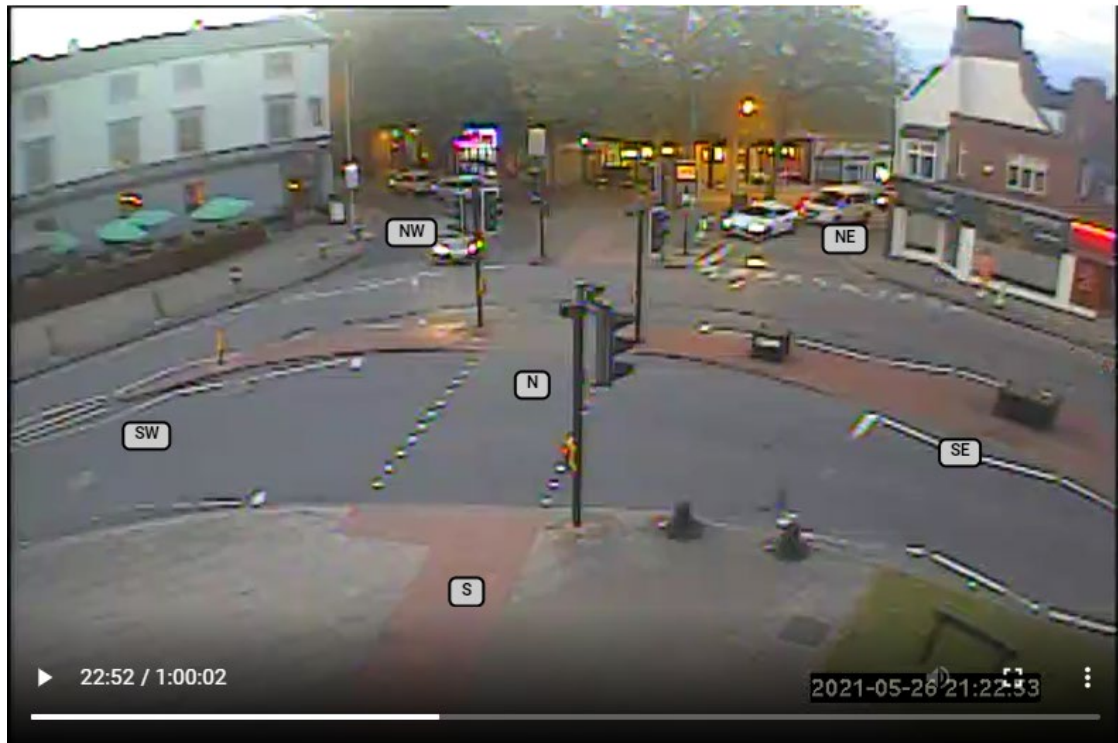


Figure 20: Vehicle observed illegally driving southbound on Bird Street before turning left onto St John Street

Sanford Street junction with Bird Street

An average of 330 vehicles per day were observed to be accessing Bird Street via Sandford Street on market days between 7am and 7pm, with 209 vehicles between 10am and 4pm. On non-market days an average of 336 vehicles per day were observed between 7am and 7pm, with 267 vehicles between 10am and 4pm (Table 2). It has been calculated that a daily average of 400 vehicles per day are accessing Bird Street junction with the car park access road, via Sandford Street or Bird Street/The Friary.

Table 2: Average daily vehicle counts Lichfield City Centre

	Market Day				Non-Market Day			
	7am-7pm		10am – 4pm		7am – 7pm		10am - 4pm	
	Vehicles per day	Delivery vehicles per day (% of total)	Vehicles per day	Delivery vehicles per day (% of total)	Vehicles per day	Delivery vehicles per day (% of total)	Vehicles per day	Delivery vehicles per day (% of total)
Tamworth Street with Conduit Street	565	15%	327	16%	562	19%	337	18%
Conduit St with Dam Street	581	15%	340	16%	579	19%	547	12%
Market St with Breadmarket St	620	17%	297	17%	602	20%	318	19%
Breadmarket St with Bore St	630	17%	299	17%	614	20%	324	20%
Bird Street with The Friary	132	36%	75	16%	110	30%	40	2%
Bore Street with St John Street	665	15%	342	15%	636	17%	369	17%
Sandford Street with Bird Street	330	-	209	-	366	-	267	-

3.4. Accident Data

In a 5-year period between January 2015 and December 2019 there was one recorded road traffic accident within the site extents. The slight injury accident occurred on Bore Street whereby a pedestrian was struck by a driver reversing their vehicle (Figure 21).

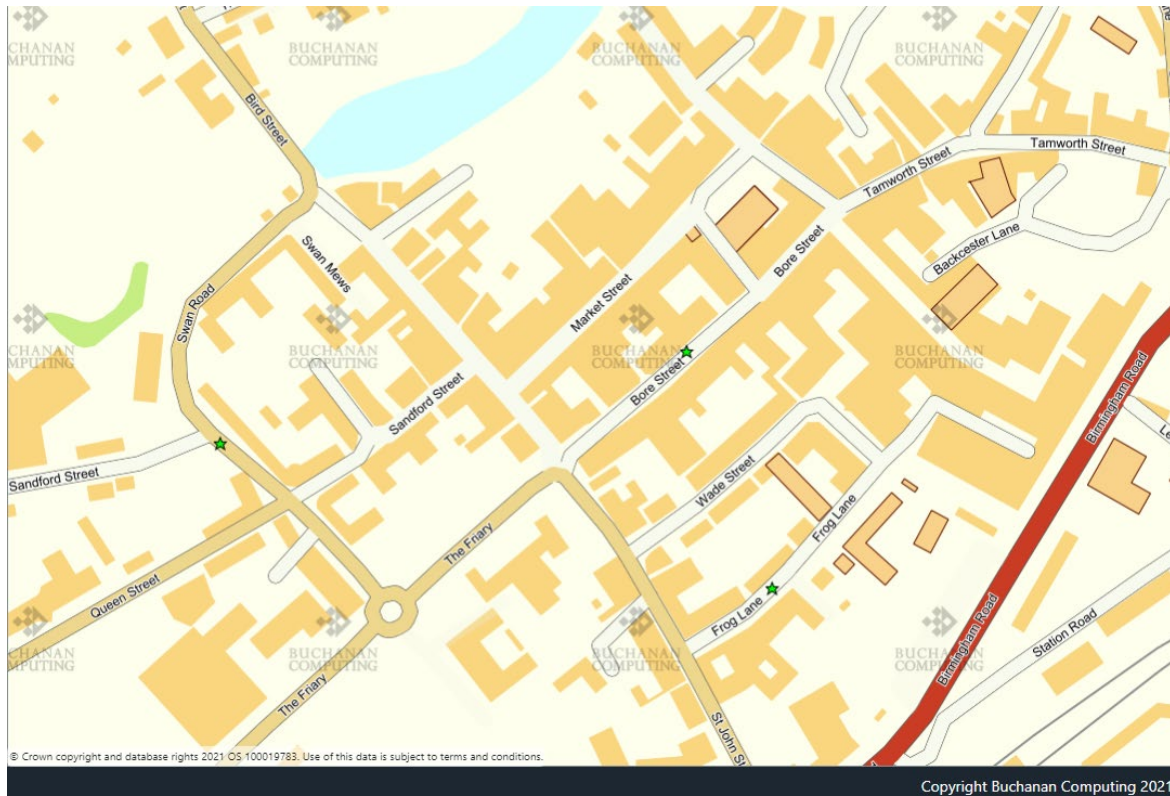


Figure 21 Lichfield City Centre Road Traffic Accidents 2015-2019

4. Site Observations

4.1. Pedestrian Zones

As discussed in 2.3.1 Lichfield City Centre is a restricted pedestrian zone. Such zones are generally shopping areas where pedestrians will normally predominate and have full use of the width of the highway extents. The current restrictions permit access for loading, disabled badge holders and permit holders resulting in an average daily flow of approximately 550 vehicles with a peak hourly flow within the restricted period of 81 vehicles (9-10am, non-market day). Traffic volumes of this magnitude would not generally be associated with a pedestrian zone as they restrict pedestrian access to the 'footway' giving greater priority to motorists. Traffic surveys indicate little change in traffic flows when the pedestrian zone is in operation which suggests a high level of noncompliance from drivers.

Following concerns from groups representing pedestrians that have loss of sight, the government issued a memorandum halting the introduction for 'shared space' schemes. The Royal National Institute of Blind People (RNIB) and The Guide Dogs for the Blind Association are currently promoting their Accessible Streets and Streets Ahead campaigns. In accordance with the Equality Act 2010 Highway Authorities are required to provide adequate guidance for those with loss of sight. Current guidance states that a carriageway should be defined by a raised kerb with a minimum height of 60mm. Where kerbs are not provided i.e. at dropped crossings suitable tactile guidance paving should be provided. Between 6pm and 9am access to the city centre is unrestricted, therefore, defined footways and carriageway should be provided. The existing low level kerbs on Tamworth Street, Conduit Street and Breadmarket Street do not conform to this guidance.

Updates to The Traffic Signs Regulations and General Directions 2016 simplified the signing requirements for pedestrian zones where cyclists are permitted with the introduction of a "PEDESTRIAN and CYCLE ZONE" sign. If any changes are to be made to the existing zone entry signage these new signs should be specified.

5. Proposals

5.1. Access Enforcement Options

The prevention of unauthorised access to any pedestrian zone relies on the use of either legal enforcement, or a physical barrier. Signage should be simplified as much as possible with the installation of updated “pedestrian and cycle zone” signs and any access and parking restriction timings should be rationalised throughout the city centre to aid compliance and successful enforcement.

It is not feasible to distinguish or quantify permitted (loading, disabled and permit holders) and non-permitted vehicle access to the pedestrian zones via the traffic survey data, however, site observations indicate a number of vehicles do use the route as a ‘cut through’ particularly the use of Bird Street, where 60-70% of recorded vehicles were passenger cars. It is also understood that the access permit scheme is no longer actively managed and there are no permit holders for Lichfield City Centre, therefore, although some of these vehicles may be entering the zone for legitimate reasons, they are technically in breach of the current traffic regulation order. There are approximately 230 commercial and residential properties within the extents of the pedestrian zone which are eligible for an access permit. Enforcement of the current or proposed restrictions will therefore reduce traffic movements within the pedestrian zone.

In order to maintain access to the pedestrian zone for permitted vehicles, the use of legal enforcement is the preferable option, however, given the limited resource available, it is doubtful that this would result in any long-term changes to driver behaviour. Fixed camera enforcement could be considered to detect, deter and disrupt vehicles should all vehicles be banned for a set period each day (e.g. 10am-4pm), providing a long-term enforcement solution. This system relies on automatic number plate recognition (ANPR) which is capable of distinguishing between registered permitted vehicles or non-permitted vehicles during its operational period. As deliveries are undertaken daily by unknown and constantly changing vehicles it is not practicable for these to be registered on an ANPR system. Similarly, disabled badges are associated to individuals rather than specific vehicles, therefore, it is not feasible to identify such users with the use of ANPR. For these reasons the use of ANPR enforcement is not practicable where access for such vehicles is permitted and the use of traditional enforcement methods remains essential during such times. Although the exact cost cannot be determined at this stage, based on previous similar schemes, installation costs are estimated at £110,000 per ANPR unit; in addition to this, there will be ongoing management and maintenance costs which may be prohibitive for a site of this nature.

The use of a physical barrier or gate is feasible where all vehicle access is prohibited for set periods at Tamworth Street, Bird Street and Sandford Street. To permit access, a human operator, either on site or within a control centre, is required should an automatic system be installed. The installation of automatic movable physical barriers, such as a rising bollard, would be costly to install and maintain. Such a system can facilitate the controlled entry of permitted vehicles either with the use of an ANPR or a key fob type system, however, as discussed above, an automated system like this this is not feasible where disabled or

loading is permitted. Should there be a desire for the barrier to operate under these access conditions then a camera and intercom system will need to be maned throughout the hours of any restrictions (this can be linked to existing council call centre operators). Although used extensively within town centres their failures are well documented with a number of systems being removed or abandoned. It should be noted that it is Staffordshire County Council policy not to take on the liability of these systems so they would therefore need to be maintained and managed solely by Lichfield District Council. An alternative solution is the use of manually operated rise/fall bollards, however, as with any inground apparatus they are prone to failure over time due to dirt and water ingress; for this reason, the use of lockable swing gates would be the preferred manually operated barrier, however would require twice daily operation and add to street clutter.

The traffic data obtained for this report has highlighted a pattern of illegal vehicle manoeuvres particularly at the junction of The Friary, Bore Street, Bird Street and St John Street (Figure 16 and 17). Consideration should be given to kerb realignment and the use of bollards to physically prevent any form of left turn out of Bore Street. The majority of these manoeuvres are believed to be taking place early morning and evening therefore in the short-term details should be passed to the community policing team who can provide targeted enforcement.

	Pros	Cons
Legal enforcements	Minimal ongoing maintenance costs	Requires police resource Ineffective
Automatic Number Plate Recognition	Effective during hours of operation Able to identify/allow access for permit holders	High ongoing costs Unable to identify disabled or delivery vehicles
Automated rising bollard/gate	Effective during hours of operation Able to identify/allow access for permit holders	High ongoing costs Inherently known to fail Human operator required to permit access for disabled or delivery vehicles
Manual gate	Effective during hours of operation Minimal ongoing maintenance costs	Ongoing operational costs No access for any users when closed

5.2. Traffic Regulation Order Access Restriction Proposals

5.2.1 Option 1 (Drg No. D4281K.R01.001)

To enhance the pedestrian environment of the city centre, additional restrictions should be introduced during times of peak pedestrian activity to prohibit all motorised vehicles from entering. It is proposed that the pedestrian zone of Tamworth Street, Bore Street, Conduit Street, Market Street and Bird Street, are restricted to all vehicles throughout core trading hours i.e. 10am – 4pm, with only delivery vehicles, permit holders and disabled badge holders permitted outside these times. Traffic survey data indicates that during core trading hours in excess of 300 vehicles a day from the Bore Street Pedestrian Zone and a further 300 vehicles a day from the Bird Street Pedestrian Zone will be prevented from entering the pedestrian zone which will improve accessibility and mobility for pedestrians.

Such proposals will also restrict access for permit holders and delivery vehicles throughout the proposed pedestrian zone period therefore further consultation with residents and business owners will be required to fully understand the implications and gain local support for such restrictions. A permit system for permitted resident and business vehicles should be reinstated to clearly identify those vehicles with a right to access the pedestrian zone.

To maintain current disabled parking provision within the city centre during core trading hours, it is proposed to reallocate existing limited waiting on-street parking spaces within Wade Street and across the city centre car parks. More detailed consideration of the most suitable location of this disabled parking should be undertaken should this option be progressed to ensure an even coverage of disabled parking provision throughout the city centre. Consideration should also be given to limiting waiting within a proportion of city centre car parks to provide regular turnover of the space available.

It should be noted that proposed restriction timings are indicative and can be amended to accommodate stakeholder requirements as required. It is advisable where the pedestrian zone is operational for a limited period, that a review of existing kerbs and tactile provision should be undertaken to ensure adequate guidance is provided to those pedestrians who have loss of sight outside of any pedestrianisation period.

Implementation of this option would require installation of manual or automatic gates or rising bollards at all entry points into the pedestrian zone (Tamworth Street, Bird Street and Sandford Street) as well as changes to the existing TRO and on street signage. Based on comparable schemes within Staffordshire it is anticipated that these works will cost approximately £30,000 (subject to confirmation of public consultation requirements)

Pros	Cons
Enhanced pedestrian environment during core trading hours	Restricted access for residents and businesses

Easily enforceable with the use of a physical barrier between 10am and 4pm	Loss of disabled parking within pedestrian zone during core trading hours
Disabled parking possible immediately adjacent to shops outside of core trading hours	Ongoing operational cost to open/close gate

5.2.2 Option 2 (Drg No. D4281K.R01.002)

Option 2 proposes to close Conduit Street, Market Street, Breadmarket Street to disabled badge holders. Loading and permit holder access will remain as existing. The section of Bore Street between Conduit Street and Breadmarket Street will be repaved to provide defined carriageway and footways delineated by a kerb to ensure inclusivity and allow vulnerable pedestrians to navigate safely. Uncontrolled pedestrian crossings will be provided on desire lines. This will become the principal route through the pedestrian zone reducing vehicle flow through the market area. Disabled badge holders will continue to be permitted to access parking spaces on Tamworth Street and Bore Street at all times. The use of Bore Street provides a direct route for vehicles through the city centre, creating potential for higher volumes of traffic using the road as a 'cut through', and therefore, should be combined with a prohibited period as discussed in Option 1. The direct alignment has the potential to increase vehicle speeds, which would need to be mitigated with traffic calming measures, such as a narrow carriageway. While this option will improve pedestrian movements within Market Square, opening the existing successful pedestrian zone of Bore Street to vehicles will create a barrier between Market Square and Three Spires shopping centre, negatively effecting in excess of 2300 pedestrian movements per day.

In combination with this option visible police enforcement will be required to ensure vehicle numbers are kept to a minimum and discourage 'cut through' traffic from using the route. A permit system for permitted resident and business vehicles should be reinstated to clearly identify those vehicles with a right to access the pedestrian zone.

It is proposed that 13no existing disabled badge holder parking spaces would be relocated to Wade Street and other car parks around the city centre. More detailed consideration of the most suitable location of this disabled parking should be undertaken should this option be progressed to ensure an even coverage of disabled parking provision throughout the city centre. Consideration should also be given to limiting waiting within a proportion of city centre car parks to provide regular turnover of the space available.

A review of existing kerbs and tactile provision should be undertaken to ensure adequate guidance is provided to those pedestrians who have loss of sight outside of the pedestrianisation period. In addition, the materials choices from the previous phase should be replicated for continuity.

Implementation of this option would require significant paving and kerbing works within Bore Street as well as changes to the existing TRO and on street signage. Based on comparable schemes within Staffordshire

and assuming a high quality material pallet to match the existing it is anticipated that these works will cost approximately £500,000 (subject to confirmation of public consultation requirements)

Pros	Cons
Enhanced pedestrian environment by significant reduction in traffic flows around Conduit Street, Market Street and Breadmarket Street.	Relies on driver compliance and legal enforcement
Access for loading and permit holders maintained	Expensive to implement with significant paving and kerbing works required
23 Disabled parking spaces remain within city centre	High vehicle flows (for a pedestrian zone) will remain or potentially increase and be diverted onto the currently restricted section of Bore Street
	Conflict with existing street trader licence agreements (Riley's Greengrocer)
	Proposed alignment potentially increases vehicle speeds
	Diminish pedestrian access between Market Square and Three Spires shopping centre

5.2.3 Option 3 (Drg No. D4281K.R01.003)

Option 3 proposes to remove all existing disabled parking from within the pedestrian zone. Access rights for disabled badge holders would be removed from Tamworth Street, Conduit Street, Market Street, Breadmarket Street and Bore Street with the pedestrian zone extended to at any time, resulting in a consistent pedestrian zone restriction within the city centre and allowing for the removal of some existing signage within the zone. Access for loading would be maintained reducing any impact on residents and businesses. Although it is not feasible to distinguish between disabled badge holders and permit holders within the traffic survey data, it is anticipated that such proposals would reduce traffic flows within the pedestrian zone by up to 80%. At present there are no registered permit holders therefore it is anticipated that a small proportion of this figure would be eligible for a permit and would continue to have the right to access the pedestrian zone under this proposed option. The use of physical barriers to prevent unauthorised access is not practicable for this option, however, removal of the disabled parking which acts an attractor will greatly reduce the need for vehicles to enter the pedestrian zone. A permit system for

permitted resident and business vehicles should be reinstated to clearly identify those vehicles with a right to access the pedestrian zone.

To maintain current disabled parking provision throughout the pedestrian zone period it is proposed to reallocate existing limited waiting on-street parking spaces within Wade Street and across the city centre car parks. More detailed consideration of the most suitable location of this disabled parking should be undertaken should this option be progressed to ensure an even coverage of disabled parking provision throughout the city centre. Consideration should also be given to limiting waiting within a proportion of city centre car parks to provide regular turnover of the space available.

A review of existing kerbs and tactile provision should be undertaken to ensure adequate guidance is provided to those pedestrians who have loss of sight; however, the removal of all non-essential traffic at all times will create a standardised pedestrian zone and considerably reduce the potential pedestrian/vehicle conflict. Reallocation of parking bays provides an opportunity for public realm improvements and promotion of 'café culture' and further street trading enhancements.

It should be noted that the proposed restriction timings are indicative and can be amended or remain in operation from 9am to 6pm if deemed appropriate.

Implementation of this option would require changes to the existing TRO and on street signage. The existing kerbing and paving alignment can remain until funds are made available to progress any desired public realm improvements, the most significant of which would be to Bore Street. Based on comparable schemes within Staffordshire it is anticipated that the initial TRO amendments will cost approximately £20,000 (subject to confirmation of public consultation requirements)

Pros	Cons
Enhanced pedestrian environment by significant reduction in traffic flows	Relies on driver compliance and legal enforcement
Access for loading and permit holders maintained	Loss of disabled parking within Pedestrian Zone
Potential to improve the public realm by reallocation of spaces away from parking provision	
Relatively cost effective to implement with potential for trial period	

5.2.4 Bird Street One Way System Alteration Options (Drg No. D4281K.R01.004)

In addition to options 1-3, consideration should be given to measures to reducing vehicle numbers on Bird Street. As discussed in the preceding options the use of physical measures is not practicable whilst maintaining access for permit holders throughout the day. Traffic surveys indicate an average of 400 vehicles a day travel along the street which is currently restricted to permit holders and loading only. Almost 300 of these vehicles gain access to Bird Street via Sandford Street. Given the number of premises fronting

Bird Street this is excessive indicating a significant number of drivers are using the route as a cut through. The average peak hourly flow is 48 vehicles per hour at 19:00 however flows are relatively evenly spread throughout the day. Between 10am and 4pm the average peak hourly flow is 39 vehicles per hour at 15:00.

Option A

To remove the potential cut through the existing one-way restriction on Bird Street, north of the junction with Sandford Street, should be reversed, as should the restriction on Sandford Street. This arrangement will permit access from either end of Bird Street with all traffic being required to exit via Sandford Street.

Option B

To remove the potential cut through, the existing one-way restriction on Sandford Street should be reversed allowing vehicles to exit from Bird Street using this route. Traffic data shows an average of approximately 350 vehicles a day access Bird Street via the Sandford Street junction with only 120 coming from the Friary junction. This indicates that a large proportion of unauthorised vehicles feel there is a benefit of using this route. It is therefore anticipated that removal of the option to use this route will greatly reduce vehicle flows on Bird Street.

Public realm enhancements to Bird Street are proposed within the Lichfield City Centre Master Plan report therefore any changes to the existing one-way system should be considered in conjunction with these proposals.

Implementation of either of these options would require changes to the existing street furniture and paving at the junction of Bird Street and Sandford Street, as well as to the existing TRO and on street signage. Based on comparable schemes within Staffordshire and assuming a high quality material pallet to match the existing it is anticipated that these works will cost approximately £80,000 (subject to confirmation of public consultation requirements)

Pros	Cons
All existing permitted loading and permit holder access to be maintained	Relatively high cost of Bird Street/Sandford Street junction realignment
Reduced vehicle flows by removal of unauthorised cut through traffic	

5.3. Pedestrian Improvements

Regardless to any traffic regulation order changes the junction of The Friary, Bore Street, Bird Street and St John Street will benefit from pedestrian improvements across the Bird Street and Bore Street approaches where significant pedestrian flows have been recorded. Dropped kerbs with tactile paving provision should be provided as near as practicable to these desire lines. These improvements should be completed as part of works to prevent illegal vehicle manoeuvres as discussed in 4.1 Access Enforcement Options

6. Street Furniture

Road Signage

Regulatory road signs will be required to inform drivers of the various restrictions (parking, one way, access etc.) throughout the site. Legal requirements for the positioning and lighting of such signs are set out in the TSRGD 2016. A full road sign review should be undertaken at the detailed design stage with all required signage being incorporated into the overall street furniture layout to minimise street clutter. Where feasible signs should be integrated into other street furniture, moved back to the building line, mounted onto lighting columns or building facades. Any signposts should be painted to match other furniture throughout the city centre with the addition of a contrasting, coloured band (150mm deep) to aid those pedestrians with loss of sight.

Bollards/Gates

Where bollards or gates are required to physically restrict vehicle access or protect structures they should be of a colour and style to match existing bollards used throughout the city. Any bollard should be a minimum of 1.0m tall with contrasting colour at the top to increase their conspicuity for partially those with loss of sight (as stated within the DfT Inclusive Mobility guidance document) and be 'socketed' to help reduce future maintenance costs should damage occur.

Cycle Parking

To encourage sustainable travel, additional secure cycle parking should be provided within the pedestrian zone. The use of simple Sheffield stands is recommended as they offer a robust and cost-effective solution. Any proposed areas identified will need to allow sufficient width so that parked bikes will not obstruct the free flow of pedestrians. If spaced at 1.2m (1.0m minimum) centres in accordance with best practice two bikes can be secured to the same rack with a range of locking positions. As with other items of street furniture cycle racks should have contrasting coloured bands to aid the visually impaired. It is recommended they have a powder coated finish to protect against chips, scratches and maximise longevity.

7. Recommendations / Summary

The most desirable option should be self-enforcing whilst remaining easily maintainable. Without significant changes to the existing pedestrian zone exclusions, it is not practicable to provide a physical means of enforcement to prevent unlawful access and reduce vehicle numbers. Traffic survey data indicates that the approximately 80% of vehicles within the pedestrian zone are private cars therefore removal of the need to access the zone by removing disabled parking and reversal of the Bird Street one way system will have the greatest effect to traffic flows by removing the reason for vehicles to enter. Therefore options 3 and 4 offer the maximum benefit to pedestrians and positive change to the pedestrian zone environment.

It is acknowledged that there are many permutations of the options discussed within this report with potential for a hybrid option including a combination of restriction proposals and enforcement options. Should this be deemed preferable then further consideration can be given to the implications of such an option.

The options discussed within this report should be carefully considered at cabinet level with the preferred option or options being progressed via considerable stakeholder and public consultation to ensure good public support.

Appendix A: Drawings

Drawing Number	Rev.	Drawing Name
D4281K.R01.001	P01	Option 1 - Proposed No Vehicle access between 10am and 4pm
D4281K.R01.002	P01	Option 2 - Removal of Disabled Badge Holder Access to Market Area
D4281K.R01.003	P01	Option 3- Removal of Disabled Badge Holder Access within Pedestrian Zone
D4281K.R01.004	P01	Bird Street One Way System Alteration Options

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Review of proposal to permanently relocate Blue Badge parking bays in Lichfield City Centre

Background

Lichfield District Council are looking at ways to improve and extend access to the pedestrianised area. As part of this they are proposing the permanent relocation of the previously quite extensive provision of on-street Blue Badge parking bays, which were temporarily relocated due to the separation requirements of the Covid pandemic. The desire is to use these bays to allow better social distancing for pedestrians using the retail and hospitality venues in the City Centre and to enhance access to the city for all visitors.

The Council contacted AccessAble to provide an independent view on the 'reasonableness' of this proposal with regard to the Council's duties under the Equality Act 2010 (formerly the Disability Discrimination Act (DDA)). The initial report was compiled in July 2020 from information provided by the Council and a further appraisal was requested in Autumn 2022 with a view to making any changes permanent. A site visit was undertaken on Saturday 8th October 2022 by Barbara Harrison (Access Consultant, AccessAble), in dry, bright conditions, to assess the feasibility of the proposals.

Background legislation and influencing factors

The Equality Act 2010 superseded the Disability Discrimination Act of 1995 (DDA) and Regulations and Codes of Practice were produced to replace the duties of the DDA (although it should be noted that many of the previous duties were integrated into the Act). The Act brought together, harmonised and in some respects extended pre-existing Equality law. It aimed to make it more consistent, clearer and easier to follow in order to make society fairer.

The Equality Act 2010 has introduced protection from three new forms of disability discrimination:

- Direct discrimination because of a disability in relation to goods, facilities and services
- Indirect discrimination, and
- Discrimination arising from disability

Direct discrimination is where a disabled person is treated less favourably than someone else because they have a disability. Indirect discrimination could apply when a policy, criterion or practice is applied or introduced to all individuals; but it has an effect that particularly disadvantages disabled people e.g. requiring a specific type of ID or a medical certificate. The Act also introduced a new form of discrimination, known as discrimination arising from disability. This occurs when a disabled person is treated unfavourably because of something connected to the disability e.g. lack of provision of information in accessible formats.

Service providers (in this case the Council) are required to make changes, where needed, to improve services for disabled customers or potential customers and the Act sets out three requirements for making reasonable adjustments:

- Adjustments to the way things are done (such as changing a policy),
- Adjustments to physical features (such as making changes to the structure of a building to improve access)
- Adjustments involving the provision of auxiliary aids and services (such as providing information in accessible formats, hearing enhancement systems, special computer software or additional staff support when using a service).

Previously adjustments to premises and to policies practices and procedures had to be made by service providers only where it would otherwise be ‘impossible or unreasonably difficult’ for a disabled person to use the service. Under the Equality Act, adjustments must be made where disabled people experience a ‘substantial disadvantage’.

Positive action – the use of positive action is voluntary and allows organisations to use a range of measures to meet the particular needs of people when they are providing goods, facilities and services. This will enable organisations to target their services to meet the needs of particular disadvantaged groups or even particular disabilities, if they wish to in order to address disadvantage, particular needs or low participation.

The provision of the extensive on-street parking bays (36) would potentially be considered as a positive action as they allow people with Blue Badges to park close to the services they wish to access.

It should be noted that the Equality Act 2010 (formerly the DDA) does not override any other legislation and that there are exemptions – Health & Safety, making contracts, providing a service to others, charging more, protecting the fundamental nature of a business or service. The Council may consider that their proposed measures would be for everyone’s health and safety but if challenged this may be for the courts to decide.

In reviewing the proposal to relocate these parking bays the needs of disabled people need to be taken into account to be able to assess whether the proposal is a ‘reasonable’ one.

This proposal came about due to the unusual circumstances following the Covid 19 outbreak and the introduction of social distancing across the UK which required a distance of 2m (1m plus from 4th July 2020) between unrelated individuals. It is anticipated that the outbreak may recur in some measure for some years to come and that, therefore, steps should be taken to provide protection for the public with minimum disruption or delay when required.

Whilst the original proposal took into account the need for social distancing it did not necessarily take into account the impact of lockdown on disabled, elderly or other mobility limited people who use the Blue Badge Scheme, some of whom may have been self-isolating or shielding for extended periods of time. Many people, including some disabled people expressed anxiety and concern at the return to ‘normal’ activities, particularly with regard to travelling and spending time outside of their homes.

The current requirements for eligibility for a Blue Badge are:

- you cannot walk at all (or cannot walk more than 50m)
- you cannot walk without help from someone else or using mobility aids

- you find walking very difficult due to pain, breathlessness or the time it takes
- walking is dangerous to your health and safety
- you have a terminal illness, which means you cannot walk or find walking very difficult
- you have a severe disability in both arms and drive regularly, but cannot operate pay-and-display parking machines
- you have a child under the age of 3 with a medical condition that means the child always needs to be accompanied by bulky medical equipment
- you have a child under the age of 3 with a medical condition that means the child must always be kept near a vehicle in case they need emergency medical treatment
- you are constantly a significant risk to yourself or others near vehicles, in traffic or car parks
- you struggle severely to plan or follow a journey
- you find it difficult or impossible to control your actions and lack awareness of the impact you could have on others
- you regularly have intense and overwhelming responses to situations causing temporary loss of behavioural control
- you frequently become extremely anxious or fearful of public/open spaces

The proposal

The Council are proposing the permanent relocation of the previously quite extensive provision of on-street Blue Badge parking bays in the pedestrianised city centre. The desire is to use these bays to allow better social distancing for pedestrians using the retail and hospitality venues in the city centre.

There would be no reduction in the number of spaces (there were (36) on-street parking bays and 36 of these would be relocated to alternative car parks around the city). The re-provision would be in Bird Street Car Park and Cross Keys Car Park. Bird Street Car Park is approximately 70m from Bird Street itself and approximately 55m from Market Street (one of the main shopping streets), Cross Keys is approximately 130m from the adjacent Market Square. They also have relatively flat access routes into the town centre or to the Cathedral. The previous on-street provision was located more conveniently for some of the services that Blue Badge holders might wish to use, although the proposed parking would provide access to the whole area.

An examination of a street map of Lichfield showing the car parks would indicate that the Cross Keys car park is at a fair distance from Market Street, however, on-site there are a number of walkways which give reasonably direct access.

Existing Parking, No Proposed Change:

Redcourt Car Park, Tamworth Street

There are three spaces in the car park, close to the entrance, The spaces are of minimum size with narrow side transfer zones and no end transfer zone.

Railway Station Car Park

There is a single space in the car park, at the furthest point from the station building. The space is less than the minimum size recommended with a narrow side transfer zone and no end transfer zone.

Bus Station Car Park

There are four spaces in the car park, close to the entrance. The spaces are clearly marked with adequate transfer zones to side and rear. Two of the spaces only benefit from a transfer zone to one side which may prove difficult depending on whether the driver or the passenger needs the transfer space.

Multi Storey Car Park

There are ten spaces in the car park, close to the entrance. The spaces are clearly marked with adequate transfer zones to side and rear.

Friary and Friary Inner Car Parks

No changes are proposed and these car parks were not visited.

Wade Street on-street parking

The single space for Blue Badge users on Wade Street is not clearly marked. It is a single on-street bay with a faded sign saying 'Disabled'. There is no dropped kerb adjacent to the space, making it of limited use. This is most likely used by people familiar with the city centre who would know the limitations.

There are other parking options close by, some privately provided, which would offer suitable alternative.

Spaces opposite Lombard Car Park

There are five spaces adjacent to the roadway on Cross Keys, opposite the entrance to the lower car park. These are below the minimum recommended size and have no end transfer zone. There is a kerb at the back of the space (with a minimal grass verge) and no dropped kerbs to allow people who park there to get onto the pavement without travelling on the roadway into traffic approaching the car park. The lack of protected space to the end of the space is especially hazardous where vehicles will be swinging into the car park entrance with traffic coming up the wooded lane.

All the spaces were occupied at the time of the visit but only two vehicles were displaying Blue Badges (this should be monitored to discourage abuse).

Recommendations:

- Mark out the space on Wade Street in accordance with guidance (including transfer zones) and install dropped kerb access to the pavement level.
- Relocate the five spaces on Cross Keys to a safe and secure position, preferably within the Lombard Car Park.

Appraisal of Proposed Changes:

Bird Street Car Park

Some of the relocated spaces have been marked out but, apparently, not all so far. 11 spaces are marked out close to the footpath between the car park and Market Street.

The spaces are well placed, full sized, have transfer zones to the sides and rear of each space and markings are clear.

It is not evident where the additional 7 spaces will be placed. It would be helpful to have clear signage showing where Blue Badge facilities are available.

Currently there is no charge for Blue Badge holders. If a charge were to be introduced changes would need to be made to payment machines to allow access.

The link path to Market Street is signposted but, to a visitor, it is not immediately evident whether access is through the Iceland store. The path is reasonably level with a good surface but might seem threatening to a mobility impaired person due to the blank walls and blind corners. The path is approximately 75m long, without any resting point. The site visit was carried out in daylight, and it was not evident how well the path would be lit in poorer light conditions.

At the junction with Market Street there is a fingerpost directing to various attractions and facilities but not to the car park which would be helpful for visitors to the area.

Street maps do not highlight the pedestrian route so visitors would be most likely to follow the much longer road layout.

Recommendations

- Provide clear direction signs to the Blue Badge parking bays
- Provide clear signage to direct pedestrians to/from the parking
- Ensure that good even lighting is provided
- Consider the provision of resting places (seating) at least every 50m

Cross Keys / Lombard Car Park

Some of the relocated spaces have been marked out but, apparently, not all so far. 12 spaces are marked out on the upper level, close to the pedestrian exit. The spaces are well placed, full sized, have transfer zones to the sides and rear of each space and the markings are clear. It is not evident where the additional 6 spaces will be placed. It would be helpful to have clear signage showing where Blue Badge facilities are available.

Currently there is no charge for Blue Badge holders. If a charge were to be introduced changes would need to be made to payment machines to allow access.

The pedestrian exit comprises stepped and ramped access. The ramp is 125cm width, which would not allow two wheelchairs to pass each other and, due to the curve, the opposite end of the ramp is not in sight as you enter. There are handrails each side, which is particularly helpful on a curved ramp.

The ramp from the upper deck is longer and steeper than best practice would recommend at a gradient of 1:8 where 1:12 is the steepest gradient for safe use, and such a ramp should be no more than 2m long between landings. The existing ramp is approximately 7 metres long with no

intermediate landings, which would make it difficult for many independent users and some carers to negotiate safely. There is a section of corduroy tactile paving at the foot of the ramp which is potentially confusing as this signifies steps or a hazard and a ramp is not regarded as such. The adjacent steps down to the lower car park level have, correctly, tactile warnings, handrails and good contrast step nosings. The vehicle ramp to the upper deck is steeper than the ramp and, being straight and into traffic, is not suitable as an alternative.

Currently there are no Blue Badge bays marked in the lower car park, but it may be possible to provide some with direct level access to the adjacent park. If Blue Badge parking is made available in the lower section there should be clear headroom of a minimum of 2.6m to allow for adapted vehicles or clear signage far enough from the entrance to prevent confusion. There is a long ramp, with intermediate landings, from pavement level to the lower car park but this too is steeper than required and may well be inaccessible to independent users of manual wheelchairs.

As both ramps are smooth surface tarmac and exposed to the weather it would be advisable to warn of icy conditions as well as providing grit. (Please note that grit can be hazardous as it is loose and may roll underfoot. If used it should be swept clear as soon as possible).

Recommendations

- Provide clear direction signs to the Blue Badge parking bays
- Provide clear signage to direct pedestrians to/from the parking
- Remove the corduroy tactile paving from the ramp
- If works are carried out in the area consider improvements to the ramps
- Consider the provision of Blue Badge spaces within the lower car park
- Provide clear signage regarding the headroom

The pedestrian pathway from Cross Keys and the Lombard Car Park to Market Street is not signposted to allow visitors to find a convenient route.

Street maps do not highlight the pedestrian routes either, so visitors would be most likely to follow the much longer road layout. The return route does not show a direction to the car park which could be confusing for people visiting the area. The route needs to be well lit and attention given to the dropped kerbs which have upstands greater than 6mm in places and no tactile warnings.

Recommendations

- The five on-street spaces at Cross Keys are not adequate as Blue Badge Parking and it is suggested that further provision is made in the Lombard Car Park with the five spaces converted for use by vans or larger vehicles. The spaces existing are well below minimum requirements and place users in a dangerous position when loading or unloading mobility aids and when accessing the pavement level.
- The walkways from both Bird Street and Lombard Car Parks should be well lit in all conditions, signposted, made evident on the street maps and, if possible, made to feel more

inviting. In particular, the section between Bird Street and Market Street, which is on a double, blind corner, could be disorienting and would benefit from signage or artwork.

- If at any time payment is required from Blue Badge holders the pay machines would need to be accessible. Where machines are placed in narrow bays between parked vehicles it is essential that there is sufficient room for a wheelchair or mobility aid user to turn without causing harm to the vehicles. A minimum turning circle is 1.5m but, without a wall barrier, at least 2m is advisable.
- The Wade Street on-street space is not adequate for Blue Badge use, being too narrow and not served by dropped kerbs.
- Monitor the use of the Blue Badge bays and ensure they are kept solely for the use of Blue Badge holders.

Conclusion

An appraisal from a map would suggest that the proposed removal of the Bore Street and other on-street spaces and their relocation to more remote car parks would be unreasonable.

Appraisal on-site shows that the pedestrian links from the car parks are good and that the distances involved are not in excess of the existing situation. It is however vital that the pedestrian routes are accessible and clearly signposted, particularly in a city which attracts so many tourists.

It is evident that the pedestrianised area extends far beyond just Market Street and that general access is good. Care must be taken to ensure that correct tactile warnings are installed as well as adequate dropped kerbs on the access routes.

It is concluded that, provided the Cross Keys roadside spaces are replaced and the undertaking to improve dropped kerb provision from that area, as well as general improvements recommended within this document the proposals are likely to be considered 'reasonable' for the city centre.

Prepared for and on behalf of Lichfield District Council by
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AccessAble

October 2022

This report has been produced from the guidance provided by the Equality & Human Rights Commission and in BS8300 - 1:2018 (External environment) and BS8300 - 2:2018 (Buildings) Design of an accessible and inclusive built environment and if or where relevant current Building Regulations, Approved Documents K and M and other best practice guidance.

This report and the guidance therein cannot ensure compliance with previous (Disability Discrimination Act 1995 and 2005) or current (Equality Act 2010) legislation. Equally, it cannot protect from potential claims of discrimination. We do not accept any responsibility for any direct or third party loss or damages as a result of this document's use.

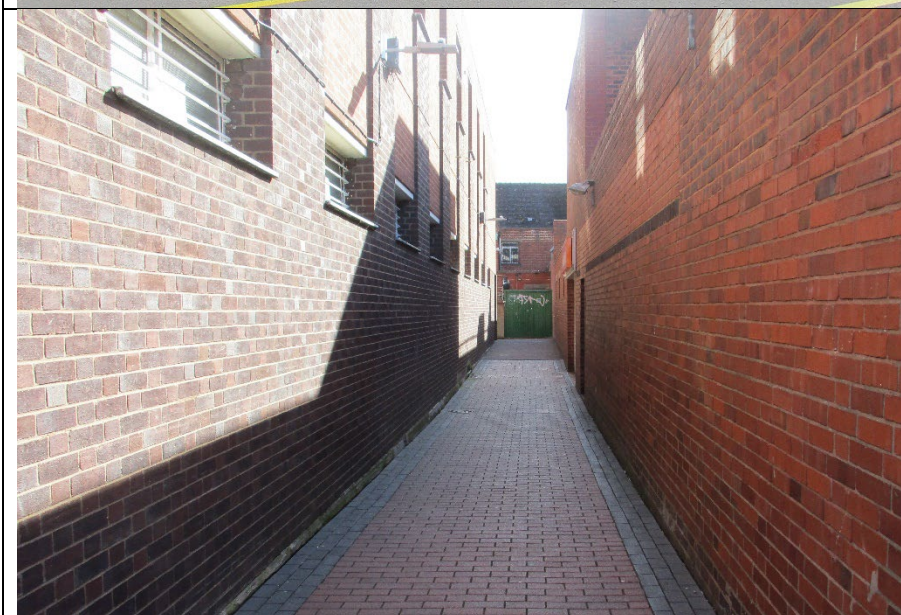
Review of proposal to relocate Blue Badge parking for Lichfield City Council



Block of accessible parking spaces in Bird Street Car Park, adjacent to pathway to Market Street.



Entrance to pathway to Market Street with signage emphasising the Iceland store.



First section of pathway with entrance to Iceland Store to the right and no indication of the continuing path



Start of the second section of the pathway showing poor surroundings, which may feel threatening to a disabled or older person



End of the second section of pathway which may appear to be a cul-de-sac to those unfamiliar with the route



Entrance to the third section of the pathway showing the blind corner which may feel threatening to a disabled or older person

Approach to Market Street

Fingerpost signage which does not mention the Bird Street Car Park.

A section of the City Map showing that the shiny surface may cause difficulties with reflections and that there is no indication that there is footpath access from either Bird Street or Cross Keys to the Market Street area





Part of the upper level of Lombard Car Park showing position of some Blue Badge Parking Bays



Lombard Car Park Blue Badge Parking Bay



Foot of the curved pedestrian ramp showing incorrect use of tactiles. There should be no tactile warning to a ramp as it is not a hazard,



Top of the pedestrian ramp from the Lombard Car Park showing the curve, meaning that people entering from the other end cannot be seen.



Lombard Car Park lower level access ramp which is long and steep but does have level landings (it is preferred that the landings contrast with the going of the ramp).



Upper section of the Lombard Street ramp from the lower level.



Five Blue Badge Bays on Cross Keys



The end of the Blue Badge Bay showing that a person unloading a mobility aid from the rear of a vehicle would be in the path of traffic.



The head of the parking bay showing there is no space and no dropped kerb



The front of a vehicle parked in the bay showing the position of the nearest dropped crossing, which would entail moving into the path of traffic



The ground alongside the bays showing no dropped kerb access and an uneven, muddy verge



The side of the bay showing insufficient space for a passenger to transfer and no dropped kerb access



The nearest dropped kerb access showing a steep slope and insufficient level landing to allow a wheelchair to turn safely



The poor dropped kerb, with an upstand and without tactiles, in the walkway to Market Street



The dropped kerb at the footpath to Market Street across the complex private car park accesses

	<p>The footpath through to Market Street</p>
	<p>The entrance to Market Street at the end of the footpath</p>
	<p>The bollard at the end of the footpath which is well placed but would benefit from some contrast in poorer light conditions.</p>

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AccessAble

October 2022

This report has been produced from the guidance provided by the Equality & Human Rights Commission and in BS8300 - 1:2018 (External environment) and BS8300 - 2:2018 (Buildings) Design of an accessible and inclusive built environment and if or where relevant current Building Regulations, Approved Documents K and M and other best practice guidance.

This report and the guidance therein cannot ensure compliance with previous (Disability Discrimination Act 1995 and 2005) or current (Equality Act 2010) legislation. Equally, it cannot protect from potential claims of discrimination. We do not accept any responsibility for any direct or third party loss or damages as a result of this document's use.

Pedestrianisation – AccessAble Recommendations

Following the report from AccessAble we have been looking to make improvements in-line with the recommendations.

We have so far:

- Installed new signage to direct people to/from the city centre car park.
- Installed new LED lighting on the footpath between Bird Street car park and Market Street.
- Rest seat installed on the footpath between Bird Street car park and Market Street.
- New clearer signage on the height restriction bar into the top layer of Lombard Street car park.
- New directional signage to direct drivers to Blue Badge parking bays at Lombard Street car park.
- Rest seat installation on the footpath from Lombard Street car park.

With regards to the recommended improvements, the council has had to strike a balance between delivering urgent items highlighted by the AccessAble report to support the trial, and the longer-term improvements needed if the pedestrianisation is made permanent.

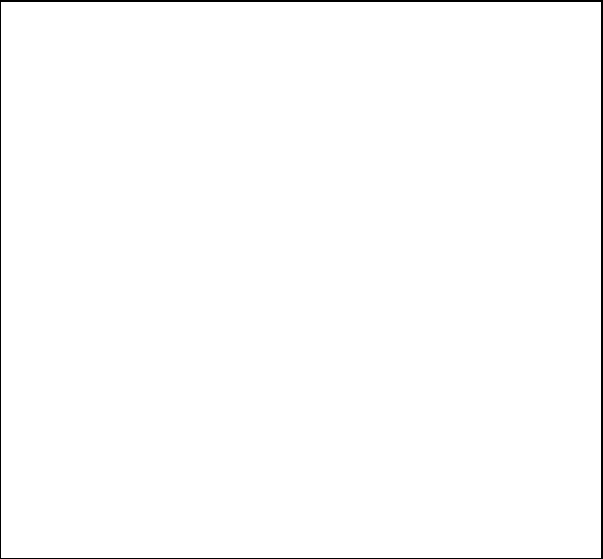
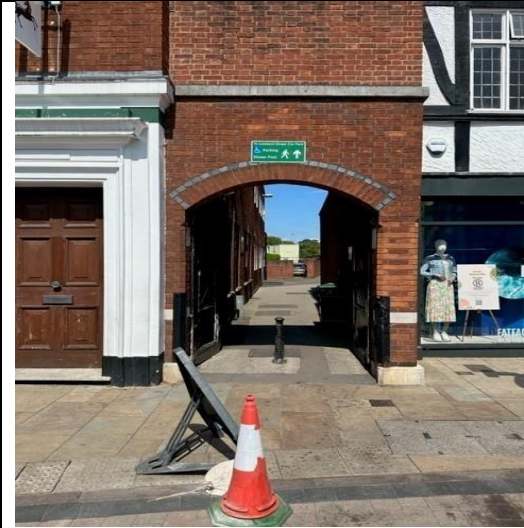
Other improvements to be considered:

- Mark out the space on Wade Street in accordance with guidance and install dropped kerb access to the pavement level.
- Monitor car park usage to determine the relocation of the five spaces on Cross Keys to an appropriate position.
- Investigate improved lighting for the walkway between Market Square and Lombard Street.

Images

Car Park Directional Signage.





Tamworth Street Gateway Signage



Bird Street to Market Street LED Lighting



Resting Points



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The implementation of an Experimental Traffic Regulation Order for the pedestrianisation of Lichfield City Centre



CLr Doug Pullen

Date: 3 November 2022

Agenda Item:

Contact Officer: John Smith - Performance and Programmes Manager

Tel Number: 01543 308016

Email: john.smith@lichfielddc.gov.uk

Key Decision? **NO**

Local Ward All

Members

Cabinet Member Report

1. Executive Summary

- 1.1 This report outlines the implementation of an Experimental Traffic Regulation Order (ETRO) on Market Street, Tamworth Street, Conduit Street, Breadmarket Street and Bore Street in Lichfield City Centre.
- 1.2 The aim is to make Lichfield city centre even more attractive to shoppers, workers and visitors, to reduce traffic movement and reduce air pollution in this locality whilst giving priority to pedestrians where appropriate.
- 1.3 A Traffic Regulation Order (TRO) is a legal order, which allows the Local Highway Authority (LHA) to regulate the speed, movement and parking of vehicles. The Act governing Traffic Orders is the Road Traffic Regulation Act 1984 and this is enforceable by law. In Staffordshire, moving Traffic Orders are enforced by the police and on-street parking restrictions are enforced by Staffordshire County Council (SCC). An Experimental Traffic Regulation Order (ETRO), is very similar to a TRO except that the Order is not permanent. Instead, an ETRO can only be in place for a maximum of 18 months. ETROs are often used by Local Highway Authorities to measure and assess the effects of new arrangements, before potentially making them permanent.
- 1.4 Parliament has provided primary and secondary legislation which prescribes how LHAs can make, amend or remove an ETRO. SCC has a process in place which meets the requirements of the relevant legislation and the authority implements hundreds of temporary and permanent Orders each year. As part of the legislative procedure for making an Order there is a requirement for consultation and for providing a process through which to consider and respond to objections.
- 1.5 Prior to and during the implementation of the proposed ETRO for Lichfield City Centre, Lichfield District Council (LDC) and Staffordshire County Council will communicate with local residents and stakeholders to ensure they are aware of the alternative provisions and mitigating measures being put in place.
- 1.6 36 Disabled Persons Parking Bays (DPPB) will be permanently relocated to Bird Street car park and Lombard Street car park as outlined by AccessAble consultancy to ensure no net loss of designated car parking for Blue Badge holders.

2. Recommendations

- 2.1 That the pedestrianisation of additional parts of the city centre is introduced through the implementation of an Experimental Traffic Regulation Order, which prevents traffic from entering the city centre from 12.00 until 21.00 every day. New restrictions would apply to Market Street, Tamworth

Street, Conduit Street, Breadmarket Street and Bore Street and incorporate existing restrictions at Dam Street and Bird Street.

- 2.2 That the Experimental Traffic Regulation Order runs for 18 months, from 1 January 2023, including a 6 month formal consultation process, and that the results of this are reported to Member Task Group and Overview and Scrutiny Committee.
- 2.3 That Bird Street car park and Lombard Street car park continue to provide for alternative Disabled Persons Parking Bays whilst the Experimental Traffic Regulation Order is in place and that these are clearly and permanently marked out to reflect their use.
- 2.4 That at the end of the Experimental Traffic Regulation Order process (18 months) the project Steering Group will need to make a recommendation on whether to make the Experimental Traffic Regulation Order permanent or to have it disregarded.

3. Background

- 3.1 The Councils' adopted Public Realm Strategy suggested that the removing of vehicles within the city centre core would be beneficial to the vibrancy and feel of the city's core. The Strategy found that due to the nature of Lichfield, the cobbled paths are very narrow and people often walked in the road.
- 3.2 The aim is to make the city centre even more attractive to shoppers, workers and visitors, to reduce traffic movement and reduce air pollution in this locality whilst giving priority to pedestrians where appropriate. People walking around this part of the city are often unaware that vehicles are permitted which may present a road safety risk. A more recent risk is through the increase in use of electric vehicles and the potential risk that they're usually much quieter operation may also present a road safety risk in city centres/pedestrian locations.
- 3.3 A separate feasibility study was prepared by Staffordshire County Council/Amey on Pedestrianised Streets for Lichfield city centre and the report suggested that the most desirable option would be for one of self-enforcing because it was easily maintainable. With traffic survey data indicating that approximately 80% of vehicles within the proposed pedestrianised zone being private cars and therefore removing the need for access would have the greatest effect on traffic flows and offer the maximum benefit to pedestrians.
- 3.4 It should be noted that within the city centre core, restrictions are already in place to reduce traffic volumes and types of user to certain times of the day and days of the week at Dam Street and Bird Street.
- 3.5 Amey completed a feasibility report in August 2021 which suggested that a single TRO be implemented for Lichfield City Centre to incorporate existing restrictions and extend the TRO to other parts of the city including Market Street, parts of Tamworth Street, Bore Street, Breadmarket Street and Conduit Street.
- 3.6 There is currently very little enforcement of the existing restrictions with contravention of a Pedestrian zone TRO being a moving traffic offence which at present is only enforceable by Police authorities. Future enforcement of new ETRO also done by Police, but by removing parking, there will be more that CEOs can do to enforce in meantime.
- 3.7 SCC have expressed an interest in obtaining enforcing powers for moving traffic offences as part of broader changes nationally which are being led by the Department for Transport. It is possible that SCC will make a formal application for the powers in late 2022, however, it is not currently envisaged that

SCC would have accessed enforcement powers before the ETRO for Lichfield City Centre completes its 18 month duration.

- 3.8 Aspirations to pedestrianise the city centre would be significantly supported by implementing the proposed ETRO. This would see the restriction of traffic every day from 12:00 until 21:00 in the designated areas.
- 3.9 An ETRO is usually in place for up to 18 months with a decision made either at the end of this period, or before, as to whether the ETRO is subsequently made permanent or removed altogether. The first six months is the formal consultation period so that feedback and objections from drivers, residents, business owners, and other stakeholders can be considered and recorded. The ETRO provides greater flexibility than a permanent TRO, for example the timing of restrictions or the geographical extents of the restrictions can be tweaked accordingly within reason. An ETRO may therefore provide a more flexible and attractive mechanism through which to explore the pedestrianisation of the city centre in Lichfield.
- 3.10 Implementing an ETRO carries an opportunity for ongoing consultation beyond the formal 6 month period and it is imperative that regular and clear communications with stakeholders, residents, Blue Badge holders and businesses take place both leading up to and during the implementation of the ETRO. A communications plan has therefore been prepared in readiness for this scheme with associated milestones and template correspondence contained within. Also contained within the Communications Plan are details of which officers in both SCC and LDC are involved with and leading on delivery of the scheme as well as any relevant contact details, in order to ensure any local representations which may stray from usual enquiry/reporting channels can easily and promptly be shared with the correct team/individual for consideration, recording and response.
- 3.11 The preferred enforcement method is the use of Automatic Number Plate Recognition (ANPR). ANPR would use fixed cameras sited at all entry points to the pedestrian zones (including Tamworth Street and Bird Street from the Friary and Sandford Street access points) which read number plates and automatically check these against an approved list of vehicles.
- 3.12 Whilst ANPR is the preferred enforcement method, the installation of camera's and an ANPR system doesn't form part of the current ETRO proposals and will only come forward if its needed as phase two. Once the ETRO has been in operation for up to 18 months, a traffic study will be undertaken to review the amount of traffic that is contravening the pedestrian zone to determine if ANPR is actually required and to formulate an expected payback period based on transgressions. If a traffic study shows that there's merit in introducing ANPR and if SCC have the delegated powers to enforce moving traffic offences, a second phase could come forward. This would be a partnership between LDC and SCC.
- 3.13 Any drivers then entering the pedestrian zone whose vehicles are not on the approved list are potentially breaching the legal Order and are potentially committing an offence, as well as potentially invalidating any vehicle insurance policy they may hold. It is the intention of SCC with the support of LDC to therefore firstly introduce a 'soft enforcement' procedure as part of the ETRO. Rather than issuing fines for first offenders and certainly during the first few weeks following implementation, those drivers who are found to be breaching the Order will initially only be issued with a warning letter, informing them that they have done so and that if they repeat the offence they may be liable to pay a fixed Penalty Charge Notice.
- 3.14 The 'soft enforcement' process is envisaged to be in place for a number of weeks whilst the initial stages of public consultation are underway and the development of the approved list of vehicles is finalised. At the earliest possible stage, agreement on a timescale for when the Order should become fully enforceable, i.e. drivers (with the exception of those who are committing a 'first-time' offence)

found to be breaching the Order will automatically be issued with a PCN, will be agreed between SCC and LDC and communicated to all Members. Once communicated to local stakeholders, the timescale for full enforcement will then also be promoted publicly in accordance with the Communications Plan and well in advance of full enforcement becoming 'live'.

- 3.15 The district council has appointed an independent company (AccessAble) to evaluate the changes being suggested for those who are Blue Badge holders and for people with disabilities in general. They concluded that with minor alterations to ensure that routes from car parks are accessible and clearly signposted, the proposals are likely to be considered appropriate and reasonable for the city centre. As part of this process an Equality Impact Assessment (Appendix A) has been developed which shows that the council has considered its proposal to introduce an Experimental rather than Permanent TRO and that the nature of the Order is flexible and as such allows for tweaks i.e. reasonable adjustments, to be made as appropriate.
- 3.16 As part of introducing the ETRO a proportion of on-road Disabled Persons Parking Bays will be removed from use. The majority of these are currently suspended due to local agreement between LDC and SCC to extend existing arrangements on site that were introduced during the COVID Pandemic. The reason for the initial suspension of the bays was primarily associated with rules around reducing the spread of the COVID-19 virus. The Government provided SCC with additional funding (Emergency Active Travel Fund (EATF)) in the summer of 2020 to help reopen town centres safely during the Covid pandemic. SCC identified Conduit Street as a pinch point for pedestrians and it was decided that temporarily suspending disabled parking bays on Conduit Street would enable additional space for pedestrians to pass each other safely.
- 3.17 Planters were placed in the suspended disabled parking bays by SCC to create space for pedestrians and to provide an attractive alternative to road cones and plastic barriers. The scheme enabled visitors and shoppers to continue to visit the city centre in a safe environment.
- 3.18 SCC agreed with LDC to also temporarily suspend disabled bays on Tamworth Street and Market Street to reduce the amount of traffic in the city centre and provide more space for pedestrians. During the pandemic the government also enabled greater flexibility in the processes associated with pavement licenses to encourage/enable business to continue to operate in towns and city centres. Whilst the EATF fund ran out during 2021, the pavement license legislation (The Business and Planning Act 2020 (Pavement Licences) (Coronavirus) (Amendment) Regulations 2021) was extended to September 2022 and more latterly extended to 30 September 2023. Responsibility for the placement and maintenance of the planters transferred to LDC in 2021 via local agreement.
- 3.19 A pavement licence permits the business to use furniture placed on the highway to sell or serve food or drink and/or allow it to be used by people for consumption of food and drink supplied from, or in connection with the use of the premises. It is envisaged that many businesses would be able to continue to operate outdoors by applying for pavement licenses up until the end of December 2022 where they intend using suspended bays, and can also apply for pavement licence with an expiry date of 30 September 2023 where they are not using suspended bays.
- 3.20 Alternative parking provision was made for Blue Badge holders as part of the EATF scheme and the intention is to continue with this alternative provision.
- 3.21 The alternative DPPBs in Bird Street and Lombard Street will be marked out permanently for the duration of the ETRO. These will have additional space either side of the bay for disabled access with the space being approximately 3.6 metres wide as opposed to 2.4 meters. The bays will also be hatched out at the rear of the bay allowing rear access from the vehicle and giving 6 metres long as opposed to 4.8 meters.

- 3.22 Blue Badge holders can also park in any bay on any district council car park free of charge if there are no DPPBs available when displaying their Blue Badge. Blue Badge holders can also park legally on single or double yellow lines within the city for up to three hours, as long as they are not causing an obstruction. The ability for Blue Badge holders to do this will form part of the Communication Plan for the pedestrianisation project.
- 3.23 The adjustments being proposed for DPPB provision as part of the ETRO have been in place for nearly two years as part of the existing arrangements introduced in summer 2020. It is therefore envisaged that the majority of Blue Badge holders visiting the city are local and have made use of, and are familiar with the alternative arrangements.
- 3.24 The taxi rank on Bore Street is a night time facility and will not be available for taxis during the restricted times between 12.00 and 21.00 each day. Taxis' will need to find alternative pick-up points such as council car parks and the train station taxi rank. There is, however, an opportunity to extend the taxi rank along Bore Street for taxi use after 21.00 to support the night-time economy.
- 3.25 There will be a number of business owners and residents who will be affected by the ETRO, as they need to drive within the city centre on a regular basis. For these individuals an approved list of vehicles will be developed and managed by Staffordshire County Council. The approved list of vehicles allows the ANPR system to recognise their vehicles and to not issue fines/warning letters. The restrictions will allow for loading outside the hours of enforcement, enabling businesses to continue to take deliveries on a daily basis.
- 3.26 The broad timelines for implementation, a project plan and a Communications Plan have been prepared. The project Steering Group includes officers from Amey, Staffordshire County Council and the District Council.
- Commence drawings, CDM documentation and associated materials for consultation, including any works orders being placed – October 2022
 - Commence key stakeholder consultation (County Councillors, emergency services, LDC, LCC, freight authorities, etc...) – approx. three weeks commencing in October
 - Confirm ETRO with Regulation team with a date to implement – late October
 - Public Engagement with businesses and residents – meeting to discuss contents of letter drop, Letter drop arranged by SCC, LDC to consider additional consultation materials based on same information and methods of distribution – October
 - Implement ETRO - notices in the press, construction works associated with Order – End Oct/Nov
 - Public Consultation undertaken for 6 months minimum (including any drop-in events, door-to-door consultation, leaflet drops) - Dec 2022 - May 2023
 - Final decision on a TRO to be made - May 2024.
- 3.27 It is fully expected that throughout the implementation of the ETRO, the City Centre Member Task Group and Overview and Scrutiny Committee will be kept informed of progress by the project Steering Group. Likewise, Members at Staffordshire County Council and Lichfield City Council will be kept informed via the same project Steering Group. At the end of the ETRO process, LDC and SCC will need to make a decision whether to make the ETRO permanent or to have it disregarded.

Alternative Options	1. To not implement an ETRO and return to business as usual from January 2023, would mean drivers will continue to travel through the city and the benefits of having a cleaner and safer pedestrianised environment would not be reached. It would also prevent many businesses from being able to apply for pavement
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	licenses in order to operate outdoors unless they made an application to suspend parking bays for themselves.
Consultation	<ol style="list-style-type: none"> 1. We have consulted with AccessAble on this decision to ensure that the council has considered suitable alternative arrangements and the needs of disabled Blue Badge holders. 2. Lichfield City Council have confirmed their in-principle support for a pedestrianised City Centre, pending further discussion and analysis of a detailed scheme proposal and its implications 3. The council's city centre masterplan Member Task Group are supportive of a pedestrianised city centre.
Financial Implications	<ol style="list-style-type: none"> 1. The cost to implement the ETRO is £27,000. The district council plan to appoint Amey through Staffordshire County Council's Infrastructure+ Partnership to carry out the implementation of the ETRO. 2. There will be a loss of income due to some bays in Bird Street car park and Lombard Street car park being made available for Blue Badge holders which will be free of charge. The maximum loss of revenue will be £50,000 in total, £38,000 in Bird Street due to a reduction of 10% paid parking bays and £12,000 in Lombard Street due to a reduction of 7% paid parking bays, however we anticipate non Blue Badge users will be displaced to other car parks within the city where an increase in income will be reflected.
Approved by Section 151 Officer	Yes
Legal Implications	<ol style="list-style-type: none"> 1. None.
Approved by Monitoring Officer	Yes
Contribution to the Delivery of the Strategic Plan	<ol style="list-style-type: none"> 1. Restricting vehicle access in the city centre supports the priority of shaping place and developing prosperity. 2. Enabling people to live healthy and active lives by allowing more pedestrianisation space within the city centre. 3. Develop prosperity to encourage economic growth by supporting businesses with their requests for pavement licenses for outdoor space.
Equality, Diversity and Human Rights Implications	<ol style="list-style-type: none"> 1. In introducing the removal of the Disabled Persons Parking Bays, the District Council engaged with an advisory body, AccessAble to determine the impact on Blue Badge holders of the proposal. 2. The Council has dedicated provision for alternative parking (with no net loss), to meet the needs of Blue Badge holders, these being facilities within Bird Street Car Park and Lombard Street Car Park both located in close proximity to shops, cafes and other facilities. 3. Blue Badge holders can also park in any bay on any council car park free of charge if there are no Disabled Persons Parking Bays available when displaying their Blue Badge. 4. Blue badge holders can also park legally on single or double yellow lines within the city for up to three hours as long as they are not causing an obstruction. 5. This dedicated provision will continue to be kept for Blue Badge holders whilst the ETRO is implemented. 6. An Equality Impact Assessment has been carried out.

Crime & Safety Issues	1. There is an opportunity to extend the taxi rank along Bore Street to facilitate taxi parking after 21.00 each evening to support the night time economy and allow people to get home more easily after an evening out in the city.
Environmental Impact	<ol style="list-style-type: none"> 1. The reduction in vehicles in the city centre will see a change in the character and appearance of those streets where cars would have previously have driven/parked. 2. The removal of vehicles will see the streets of the city centre become cleaner with much less exhaust fumes and with cleaner air. This will make it a safer, healthier and friendlier environment for pedestrians. 3. Pedestrianisation will benefit many businesses who can continue to trade outside their buildings demise through the use of pavement licenses, allowing visitors an opportunity for outdoor dining.
GDPR / Privacy Impact Assessment	Not applicable

	Risk Description & Risk Owner	Original Score (RYG)	How We Manage It	Current Score (RYG)
A	Managing business and Blue Badge holders expectations	Yellow	Ensure clear communications to business and local residents as part of the changes – ahead of any changes.	Likelihood Yellow Impact Yellow Overall Yellow
B	Disabled Persons Parking Bays within car parks are misused or made unavailable in other ways	Green	Ensure good communications in advance and also where practical introduce new signage and physical means of demarcating the spaces available to Blue Badge holders.	Green Likelihood Green Impact Yellow Overall Green
C				
D				
E				

	Background documents Any previous reports or decisions linked to this item
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	Relevant web links Lichfield city centre regeneration projects – Public realm strategy (lichfielddc.gov.uk)
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ROAD TRAFFIC REGULATION ACT 1984

STAFFORDSHIRE COUNTY COUNCIL

Various Roads, Lichfield City Centre (Prohibition of Motor Vehicles) and The Staffordshire County Council (Lichfield) (Prohibition and Restriction of Waiting and Loading and Parking Places) (Consolidation) Order No 1/2009

(Amendment)

No. 49/2023 Experimental Order 2023

1. Staffordshire County Council on 28th February 2023 made an Experimental Order under the Road Traffic Regulation Act 1984 to introduce prohibition of motor vehicles except for loading between the hours of 9pm and 12pm the following day and No Waiting at Any Time on the following roads in Lichfield city centre: Bird Street, Bore Street, Breadmarket Street, Tamworth Street, Conduit Street, Market Street, Sandford Street, Dam Street.
2. This Order hereby revokes that part of the prohibition of vehicles between the hours of 9am and 6pm Order insofar as it relates to the Schedule to this Order.
3. Documents may be examined during office hours at 1 Staffordshire Place, Tipping Street, Stafford, ST16 2DH and during opening hours at Lichfield Library, The Guild of St Mary's Centre, Market Square, Lichfield, Staffordshire or at www.staffordshire.gov.uk/trafficregulationorders.
4. The Order will commence on 3rd March 2023 and will continue in force for up to 18 months.
5. The Council will be considering in due course whether the provisions of the Order should be continued indefinitely. Objections against the Order being continued indefinitely should be sent in writing to: Strategic Asset and Network Management, Highways, 2 Staffordshire Place, Tipping Street, Stafford, ST16 2DH or to trafficregulationorderobjections@staffordshire.gov.uk within six months of the Order or modification to the Order coming into operation, quoting reference HoCPS/TM/TR42/22 and stating the grounds on which they are made.

2nd March 2023

John Tradewell, Director of Corporate Services,
1 Staffordshire Place, Tipping Street, Stafford, ST16 2DH.

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Pavement Café Survey issued August 23 – September 4 2022.

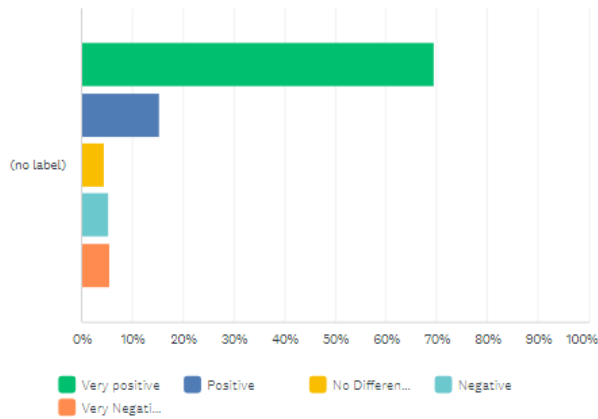
1592 responses

Q1

Customize Save as

Do you think the pavement cafes positively add to the city centre experience in Lichfield?

Answered: 1,588 Skipped: 4



	VERY POSITIVE	POSITIVE	NO DIFFERENCE	NEGATIVE	VERY NEGATIVE	TOTAL	WEIGHTED AVERAGE
(no label)	69.46% 1,103	15.37% 244	4.41% 70	5.23% 83	5.54% 88	1,588	1.62

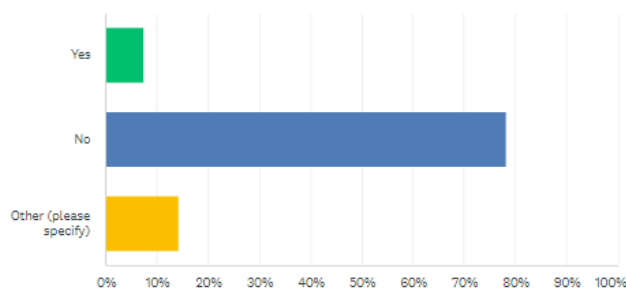
Comments (427)

Q2

Customize Save as

Have you been impacted by the pavement cafes using blue badge parking bays? Please use the comment box to tell us how this has affected you.

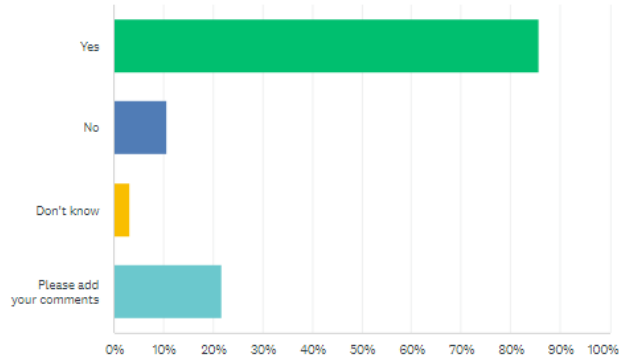
Answered: 1,589 Skipped: 3



ANSWER CHOICES	RESPONSES
Yes	7.36% 117
No	78.29% 1,244
Other (please specify)	Responses 14.35% 228
TOTAL	1,589

Do you want the pavement cafes to continue to operate as long as disabled parking is still available in the area?

Answered: 1,592 Skipped: 0



ANSWER CHOICES	RESPONSES
Yes	85.62% 1,363
No	10.62% 169
Don't know	3.20% 51
Please add your comments	Responses 21.67% 345
Total Respondents: 1,592	

Engagement since January 2023 has taken place by social media, press releases, posters and resident and business e-bulletins.

Social Media – Facebook and Twitter and LinkedIn

14th July Pedestrianization Survey

7th July Pedestrianization Business Specific Session - Announced

16th June Pedestrianization Feedback Session - Further Promotion

12th June Pedestrianization Feedback Session - Announced

12th May Pedestrianization Workshop - Feedback Shared

11th May Pedestrianization Workshop - More than 100 people attended

11th April Pedestrianization Workshop -Invite to Rearranged Session

28th February Pedestrianization Drop-in Session - Further Promotion

24th February Pedestrianization Drop-in Session - Announced

17th February Pedestrianization Workshop (Original) - Announced

Pedestrianisation Survey

Post Type: Image posts with long caption containing hashtags

Lichfield District Council

Lichfield District Council have launched a new survey to gather valuable feedback on the trial pedestrianisation scheme in the city centre.

As part of an ongoing consultation with residents and businesses, the survey seeks opinions on the scheme which was implemented in March.

You can fill the survey in online here: <https://bit.ly/3ObEQyf>
Or alternatively paper copies are available at the council's reception desk located in Frog Lane.

#LichfieldDistrict #BurntwoodCommunity #lichfieldCommunity



We have launched a new survey to gather valuable feedback on the trial pedestrianisation scheme in the city centre.

+ Manage Tags

Pedestrianisation Business Specific Sessi...

Post Type: Image posts with long caption containing hashtags

Lichfield District Council

#Lichfield business principals are invited to register for an extra feedback session from Lichfield District Council next week focussing on the pedestrianisation of the city centre.

It is an opportunity to hear from councillors and project officers in the light of responses received as part of the ongoing consultation that will help inform the future of the pedestrianisation scheme.


The session, on Thursday 13 July at District Council House in Frog Lane from 6pm to 7pm, will allow discussion to be more targeted on the issues concerning businesses.

It will run as an addition to the two sessions taking place on Monday 10 July at Darwin Hall, from 6pm to 7pm and 7.15pm to 8.15pm.

Anyone wishing to attend business-focused meeting on Thursday, or who is already registered on one of the Monday sessions and would like to swap, is asked to please complete a short online form.

<https://tinyurl.com/mspnc57z>

#LichfieldCommunity #Burntwood



Additional pedestrianisation feedback session scheduled for city businesses

Lichfield district council

Pedestrianisation Feedback Session - Pac...

Post Type: Image posts with long caption containing hashtags

Lichfield District Council

Places are available at two pedestrianisation feedback sessions taking place in #Lichfield next month.


The sessions are an opportunity to consider the responses received as part of the consultation on the pedestrianisation of the city centre which has been running since January.

Two sessions are being held at Darwin Hall, Heathcot Place to cater for as many people as possible on Monday 10 July 2023 from 6pm to 7pm and 7.15pm to 8.15pm.

They will be attended by Lichfield District Council Leader Doug Pullen and other members of the authority, and a public address system and screens will be available.

To apply for a place at the sessions, which will be allocated on a first come first booked basis, visit: <https://tinyurl.com/mjp6uvxu>

#LichfieldCommunity #Burntwood



Places available for next month's feedback sessions on Lichfield city centre pedestrianisation scheme

+ Manage Tags

Pedestrianisation Workshop Feedback

Post Type: Image posts with long caption containing hashtags

Lichfield District Council


We are very grateful to the people who came along to Darwin Hall last month for the workshop focussing on the #Lichfield city centre pedestrianisation trial.

Numerous ideas and suggestions were made for adapting the experimental traffic regulation order (ETRO) which is in place and they are available for review at <https://bit.ly/3O5WLqv>.

This is not an exhaustive list and all comments that have been submitted, both at the workshop and during the trial will be considered.

A second follow up meeting, attended by Council Leader, Cllr Doug Pullen, is being scheduled and we will release more information about this once the location and date is confirmed.

#Lichfieldcommunity #Burntwood



The feedback from last months pedestrianisation workshop has been shared by Lichfield District Council

Lichfield district council

+ Manage Tags

Pedestrianisation Workshop Held
 Post Type: Image posts with long caption containing hashtags



Last month more than 100 people attended workshops in the city, hosted by #Lichfield District Council's Chief Executive Simon Fletcher, to share their views about the pedestrianisation of the centre of Lichfield.

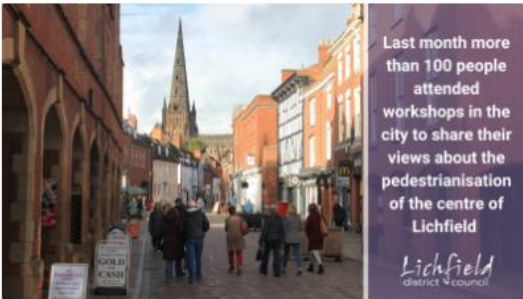
The workshop was created in response to concerns raised by businesses and blue badge holders with council officers on hand to listen to people's worries and various positive options for refining the scheme, that were suggested.

A second workshop, which the Leader of Lichfield District Council will attend, will be scheduled shortly and invitations to this will be sent out to everyone who attended last month's workshop and to anyone else in the district who would like to attend.

Information relating to these workshops will be shared via email to registered attendees and those who have requested information, on the Lichfield District Council website, on our social media, shared with local and regional press contacts and in local printed publications where the timing allows.

For more information, please visit: <https://bit.ly/42KOjkr>

#Lichfieldcommunity #Burntwood



+ Manage Tags

Pedestrianisation Guildhall Drop In
 Post Type: Image posts with long caption containing hashtags



#Lichfield District residents are invited to a drop-in session to share their thoughts on the pedestrianisation of the city centre.

The Guildhall (The Guildroom)
 Wednesday 1 March
 12noon to 4.30pm

We look forward to meeting you and discussing constructive ideas together.

#Lichfieldcommunity #Burntwood

Pedestrianisation Guildhall Drop In Tomorrow...
 Post Type: Image posts with long caption containing hashtags



#Lichfield residents and business owners are reminded there is an open drop-in event tomorrow, Wednesday 1 March, where they can share their thoughts on the trial pedestrianisation of the city centre.

The Guildroom in The Guildhall, Bore Street
 12noon to 4.30pm

The restrictions are being introduced to make Lichfield city centre safer and more attractive to visitors and is being trialled until Summer 2024 through an experimental Traffic Regulation Order.

<https://bit.ly/3SCXKys>

#Lichfieldcommunity #Burntwood

Pedestrianisation Guildhall Drop In
 Post Type: Image posts with long caption containing hashtags



#Lichfield District residents are invited to a drop-in session to share their thoughts on the pedestrianisation of the city centre.

The Guildhall (The Guildroom)
 Wednesday 1 March
 12noon to 4.30pm

We look forward to meeting you and discussing constructive ideas together.

#Lichfieldcommunity #Burntwood

Pedestrianisation March 6 Event
 Post Type: Image posts with long caption containing hashtags



Blue Badge holders and city centre business owners are being invited to attend a workshop with District Council Leader Doug Pullen and members of Staffordshire County Council to discuss the pedestrianisation of the city centre and to share ideas and suggestions for any possible refinements to the scheme. Boley Park Community Hall Thursday 9 March from 6pm to 7.15pm To make this as productive a session as possible, representatives for blue badge holders and city centre businesses are asked to register as soon as possible at <https://bit.ly/3lw3UUG> Due to the size of the venue capacity is limited to 100 people so registrations will be taken on a first come first served basis. #Lichfield #Burntwood #Lichfieldcommunity



+ Manage Tags

Pedestrianisation Quick Survey

Post Type: Long text posts containing hashtags asking a question



Have your say on the trial pedestrianisation 🚶🚲

Next month Market Street, Tamworth Street, Conduit Street, Breadmarket Street and Bore Street roads will be formally restricted to vehicles from 12noon until 9pm every day and will join the existing restrictions in Dam Street and Bird Street.

The restrictions, which are being trialled until Summer 2024 through an experimental Traffic Regulation Order, will make it safer for people to walk around the city and aim to both improve air quality and promote sustainable travel to further help the environment.

🗳️ Are you in favour or not? Every opinion matters so let us know what you think by visiting:

#Lichfieldcommunity #Lichfieldcity #Lichfield #Burntwood

+ Manage Tags

Etro Introduction

Post Type: Image posts with long caption containing hashtags



#Lichfield District Council continues to work with Staffordshire County Council to make the city centre an attractive, safe, and vibrant place and confirms that from 1 January 2023 some roads will be pedestrianised in an 18-month trial.

More information: <https://bit.ly/3UJyp6n>

#Lichfieldcommunity #Burntwood



+ Manage Tags

Pavement Cafe Survey

Post Type: Image posts with long caption containing hashtags



#Lichfield District Council is seeking residents' views on the pavement cafés operating in Lichfield city centre.

You can take the survey by visiting: <https://bit.ly/3pTslFa>

#Lichfieldcommunity #Burntwood



+ Manage Tags

Blue Badge Suspended

Post Type: Image posts with long caption containing hashtags



A recent survey seeking opinion on #Lichfield's pavement cafes has drawn a positive response from the majority of people questioned.

Pavement cafes will continue to operate after 85% of nearly 1,600 respondents were in favour.

Lichfield District Council will, however, balance this with the needs of disabled residents and visitors.

Although some parking bays have been moved to accommodate pavement cafes, alternative blue badge parking spaces have been allocated in the city centre at Bird Street Car Park and Lombard Street Car Park.

For more information visit: <https://bit.ly/3Sw80Yx>

#Lichfieldcommunity #Burntwood



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Resident Bulletins

Campaign	Recipients	Opened Info	Sent
LDC News - Pedestrianisation Feedback Business Specific Session Tonight - July 2023 Pedestrianisation	6	83.30%	13-Jul-23
LDC News - Pedestrianisation Feedback Business Session - July 2023 Pedestrianisation	51	62.70%	11-Jul-23
LDC News - Pedestrianisation Feedback Session Tonight - July 2023 Pedestrianisation	103	76.70%	10-Jul-23
LDC News - Pedestrianisation Feedback Business Specific - July 2023 Pedestrianisation	107	73.80%	06-Jul-23
LDC News - Pedestrianisation Feedback Session Early Session - July 2023 Pedestrianisation	70	87.10%	04-Jul-23
LDC News - Pedestrianisation Feedback Session Late Session - July 2023 Pedestrianisation	47	84.80%	04-Jul-23
LDC News - Pedestrianisation Invitation - June 2023 Pedestrianisation	1	100%	22-Jun-23
LDC News - Pedestrianisation- June 2023 Pedestrianisation	29,418	50.50%	16-Jun-23
LDC News - Pedestrianisation Feedback Session - June 2023 LDC News Pedestrianisation	92	86.80%	14-Jun-23
LDC News - Pedestrianisation Workshop Notes Shared - May 2023 Pedestrianisation	73	93.10%	12-May-23
LDC News - Pedestrianisation Workshop Thank you - April 2023 Pedestrianisation	73	87.50%	19-Apr-23
LDC News - Pedestrianisation Workshop Tonight - April 2023 Pedestrianisation	73	90%	17-Apr-23

LDC News - Pedestrianisation Late Session - April 2023	17	82.40%	14-Apr-23
Pedestrianisation			
LDC News - Pedestrianisation Early Session - April 2023	51	94%	14-Apr-23
Pedestrianisation			
LDC News - Pedestrianisation Late Session - April 2023	13	84.60%	05-Apr-23
Pedestrianisation			
LDC News - Pedestrianisation Early Session - April 2023	37	88.60%	05-Apr-23
Pedestrianisation			
LDC News - Pedestrianisation Workshop Rearranged - March 2023	92	92.30%	30-Mar-23
LDC News			
Pedestrianisation			
LDC News - Pedestrianisation Workshop Postponed - March 2023	44	61.40%	09-Mar-23
LDC News			
Pedestrianisation			
LDC News - Pedestrianisation Workshop Postponed - March 2023	92	86.80%	09-Mar-23
LDC News			
Pedestrianisation			
LDC News - Pedestrianisation Workshop Weather Update - March 2023	92	84.60%	08-Mar-23
LDC News			
Pedestrianisation			
LDC News - Pedestrianisation Workshop - March 2023	92	86.80%	08-Mar-23
LDC News			
Pedestrianisation			
LDC News - Pedestrianisation Confirmation - March 2023	17	88.20%	08-Mar-23
LDC News			
Pedestrianisation			
LDC News - Pedestrianisation Confirmation - March 2023	3	100%	07-Mar-23
LDC News			
Pedestrianisation			
LDC News - Guildhall Drop-In - February 2023	28,064	51.70%	24-Feb-23
LDC News			
Pedestrianisation			

LDC News - Pedestrianisation Waiting List - February 2023	71	93%	21-Feb-23
LDC News			
Pedestrianisation			
<hr/>			
LDC News - Pedestrianisation Two Places Booked - February 2023	5	100%	21-Feb-23
Pedestrianisation			
<hr/>			
LDC News - Pedestrianisation One Place Booked - February 2023	90	89.70%	21-Feb-23
Pedestrianisation			
<hr/>			
LDC News - Pedestrianisation Workshop - February 2023	27,963	56.70%	17-Feb-23
Pedestrianisation			

Printed Collateral

WE WELCOME YOU TO SHARE YOUR THOUGHTS ON PEDESTRIANISATION

Come along to our drop-in session
Wednesday 1 March
at The Guildhall (The Guildroom)
Bore Street, Lichfield
The session will run from 12noon to 4.30pm

Your opinion matters and will help
to shape the future of pedestrianisation
in Lichfield City Centre.

Please go to www.staffordshire.gov.uk/lichfieldpedzone
to have your say or follow
the link on the QR code.



FIND OUT MORE ABOUT THE TRIAL PEDESTRIANISATION IN LICHFIELD CITY CENTRE

All traffic is restricted in this area during
the trial pedestrianisation taking place
from March 2023 until August 2024.

Delivery access is available 9pm
to 12noon every day.

Your opinion matters and
will help to shape the future of
pedestrianisation in Lichfield City Centre.

Please go to
www.staffordshire.gov.uk/lichfieldpedzone to have
your say or follow the link on the QR code.



Blue badge parking is available in Bird
Street and Lombard Street car parks and
blue badge holders can park for free in
any council owned parking bay.



If you have any comments
please call customer services
on 01543 308000, or follow
the link on the QR code for
more information and to
leave feedback.



CHANGES TO PEDESTRIANISATION IN LICHFIELD CITY CENTRE

Traffic restrictions do apply
in this area every day.



We are trialling pedestrianisation in line with the city centre master plan. The strategy aims to make the city centre an attractive, safe, and vibrant place for people to live, work and visit, whilst also promoting sustainable travel to help our environment.



Market Street, Tamworth Street, Conduit Street, Breadmarket Street, Bore Street will be restricted to vehicles at all times, with exceptions for loading from 9pm to 12 noon. They will incorporate existing restrictions in Dam Street, Sandford Street and Bird Street.

Permits can be obtained by residents and businesses with access to off road facilities at all times. Please email etopermits@lichfielddc.gov.uk



PEDESTRIANISATION TRIAL IN LICHFIELD CITY CENTRE

Thank you to everyone who attended the recent workshop to share their views on the pedestrianisation of the Lichfield city centre, held in response to concerns raised by businesses and blue badge holders.

Market Street, Tamworth Street, Conduit Street, Breadmarket Street and Bore Street will be restricted to vehicles at all times, with exceptions for loading for businesses from 9pm to 12noon and access to parking facilities within the trial area. Existing restrictions in Dam Street, Sandford Street and Bird Street will be incorporated.

Visit www.lichfielddc.gov.uk/pedestrianisation for workshop feedback.

Invitations to a second workshop will be sent out to everyone who attended last month's workshop and to anyone else in the district who would like to attend. Event details to follow.

Submit further comments online at www.lichfielddc.gov.uk/pedestrianisation by email to martin.gritt@lichfielddc.gov.uk, call 01543 308000 or write to Pedestrianisation Scheme Views, Lichfield District Council, Frog Lane, Lichfield, Staffs, WS13 6YU.



Use the QR code to find out more

PEDESTRIANISATION TRIAL IN LICHFIELD CITY CENTRE

Traffic restrictions do apply in this area every day.

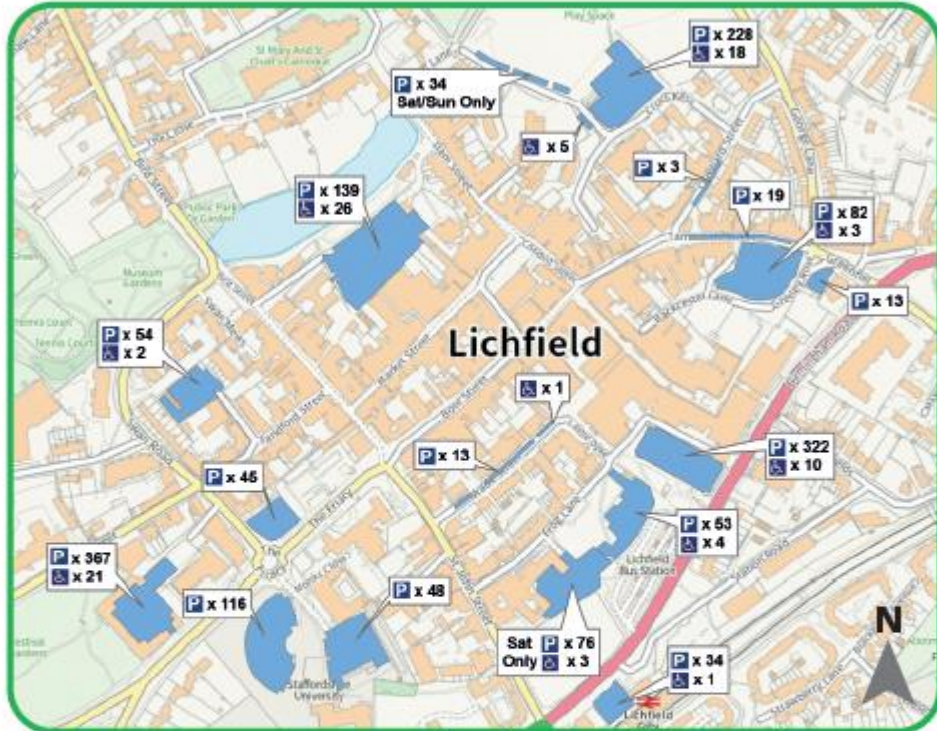
We are trialling pedestrianisation in line with the city centre master plan. The strategy aims to make the city centre an attractive, safe, and vibrant place for people to live, work and visit, whilst also promoting sustainable travel to help our environment.

Market Street, Tamworth Street, Conduit Street, Breadmarket Street, Bore Street will be restricted to vehicles at all times, with exceptions for loading from 9pm to 12 noon. They will incorporate existing restrictions in Dam Street, Sandford Street and Bird Street. Permits can be obtained by residents and businesses with access to off road facilities at all times. Please email etopermits@lichfielddc.gov.uk

Blue badge parking is available in Bird Street and Lombard Street car parks and blue badge holders can park for free in any council owned parking bay. Please see the map overleaf showing the car parks.

Use the QR code to find out more

PARKING IN LICHFIELD CITY CENTRE



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If you have any comments please call customer services on 01543 308000, or follow the link on the QR code for more information and to leave feedback.

Lichfield
district council

June 2023

Pedestrianisation Feedback Survey ongoing.

2076 responses to date and still open

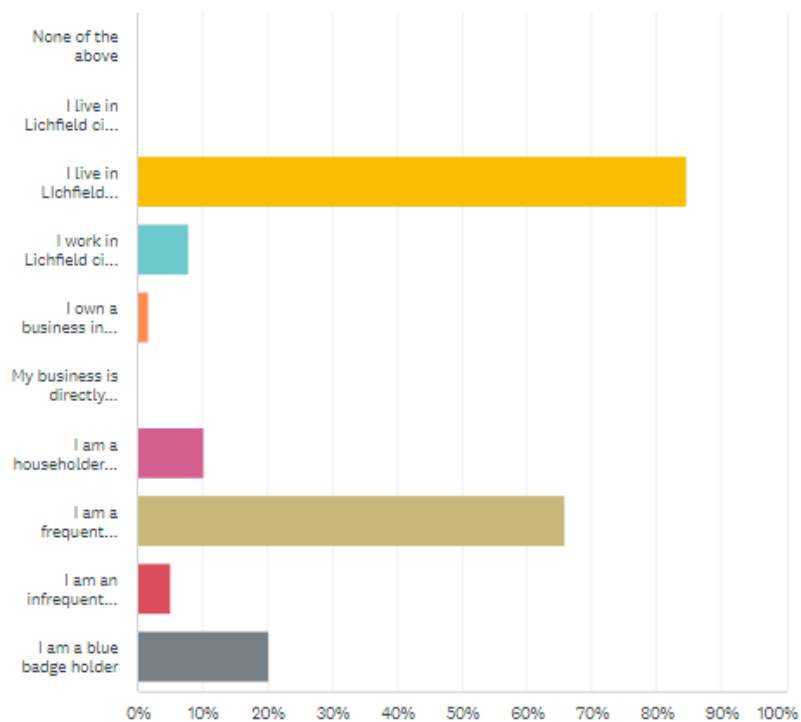
[Link to the survey results](#)

Q2

Customize Save as

Please tick which of these describe you. Please tick all that apply.

Answered: 2,074 Skipped: 1



ANSWER CHOICES	RESPONSES
None of the above	0.10% 2
I live in Lichfield city centre	0.05% 1
I live in Lichfield District	84.67% 1,756
I work in Lichfield city centre	7.91% 164
I own a business in Lichfield city centre	1.69% 35
My business is directly affected by the pedestrianisation trial	0.00% 0
I am a householder affected by the pedestrianisation trial	10.22% 212
I am a frequent visitor to Lichfield city centre	65.81% 1,365
I am an infrequent visitor to Lichfield city centre	5.11% 106
I am a blue badge holder	20.20% 419
Total Respondents: 2,074	

Q3

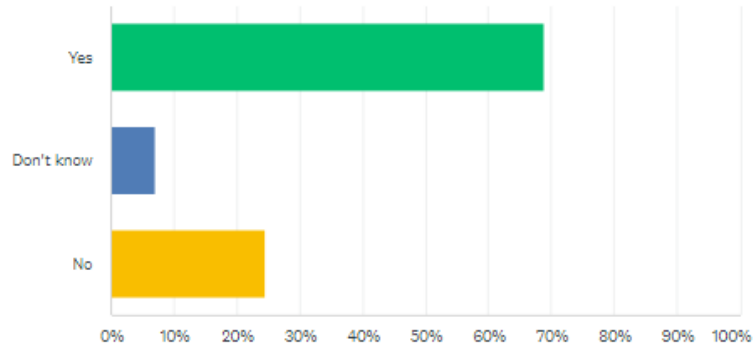


Customize

Save as

Have you noticed a change in the city centre since pedestrianisation has been in place?

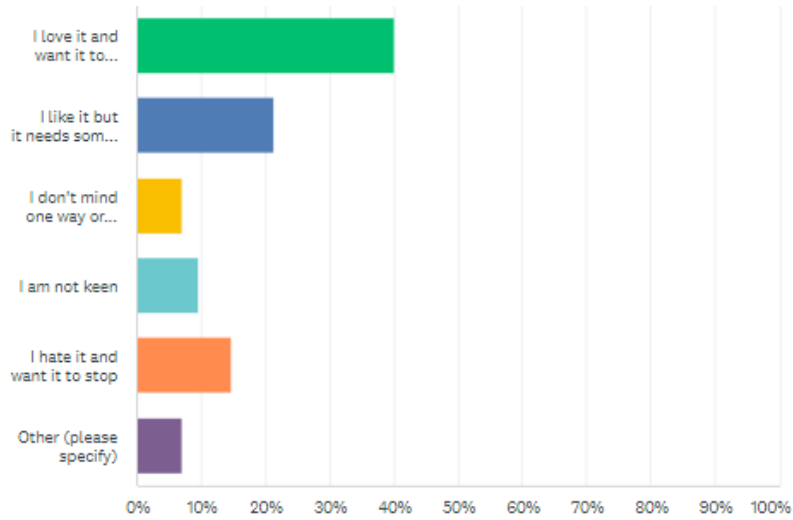
Answered: 2,070 Skipped: 5



ANSWER CHOICES	RESPONSES
Yes	68.94% 1,427
Don't know	6.96% 144
No	24.49% 507
Total Respondents: 2,070	

How would you describe your feelings about the pedestrianisation of the city centre?

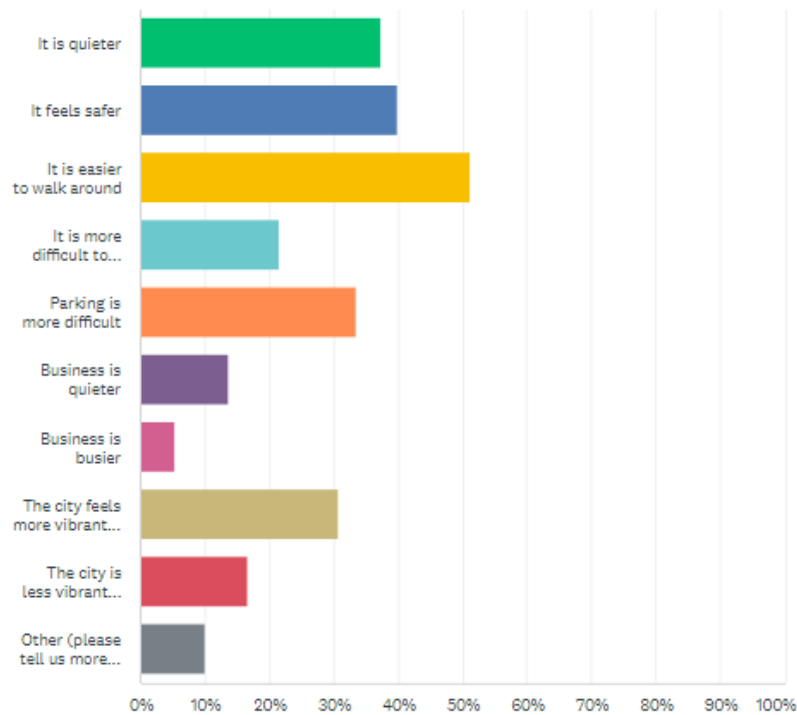
Answered: 2,072 Skipped: 3



ANSWER CHOICES	RESPONSES	
I love it and want it to continue	40.06%	830
I like it but it needs some amendments	21.43%	444
I don't mind one way or another	7.00%	145
I am not keen	9.60%	199
I hate it and want it to stop	14.77%	306
Other (please specify)	7.14%	148
TOTAL		2,072

What changes have you noticed in the city centre over the last few months?

Answered: 2,005 Skipped: 70

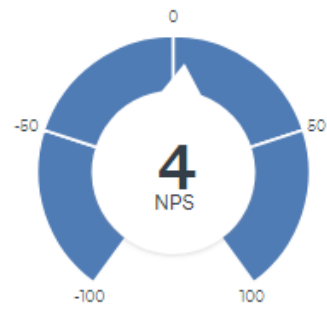


ANSWER CHOICES	RESPONSES
It is quieter	37.41% 750
It feels safer	39.95% 801
It is easier to walk around	51.12% 1,025
It is more difficult to get into the centre	21.60% 433
Parking is more difficult	33.47% 671
Business is quieter	13.72% 275
Business is busier	5.24% 105
The city feels more vibrant and welcoming	30.62% 614
The city is less vibrant and welcoming	16.66% 334
Other (please tell us more in the box below)	9.98% 200
Total Respondents: 2,005	



How likely is it that you would recommend visiting Lichfield city centre to a friend or colleague?

Answered: 2,041 Skipped: 34



DETRACTORS (0-6)	PASSIVES (7-8)	PROMOTERS (9-10)	NET PROMOTER [®] SCORE
35% 721	26% 523	39% 797	4

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Review of Overview & Scrutiny Function - Interim

Leader of the Council

Date: 3 August 2023
 Contact Officer: Christine Lewis
 Tel Number: 01543 308002
 Email: christine.lewis@lichfielddc.gov.uk
 Key Decision? NO



**Overview &
Scrutiny
Committee**

1. Executive Summary

- 1.1 In 2020 the then Strategic (Overview & Scrutiny) Committee created a Member Task Group to undertake the review and investigate options to create an effective Committee System focusing on the function of Overview & Scrutiny.
- 1.2 The Task Group compiled a number of recommendations for Strategic (Overview & Scrutiny) to consider and these were agreed unanimously to be put to Cabinet and Council where they were approved.
- 1.3 The main change was to move from a four O&S Committee structure that looked at separate areas of the organisation to one overarching Committee with a greater use of Member Task Groups which would undertake more in depth investigations into matters as and when required.

2. Recommendations

- 2.1 That Overview & Scrutiny give initial views on the effectiveness of the changes to the Overview & Scrutiny function;
- 2.2 That the Committee give a steer on the need and membership of current Member Task Groups; and
- 2.3 That the Committee give guidance as to a terms of reference to a full review of the O&S function which will be carried out at the end of this municipal year.

3. Background

The expected outcome of moving to a one O&S Committee function

- 3.1 When the original Task Group concluded to move to a one Committee structure, they expected it to create the a number of outcomes which is listed at 3.2 along with initial data.

An oversight of all Council functions by one team of members with no risk of overlap of work as seen with the old structure

There has been no overlap of work of O&S since moving to the one Committee structure. The advantage of having one O&S Chair also ensures that the work programme of both the Committee and any Task Group is monitored and efficient use of Member time is priority.

The number of formal committee meetings reduced therefore a more efficient use of resources.

The number of O&S Committee Meetings for 2020/21 (the final year of a four committee structure) was 17 with 13 task group meetings. There were also two coordination meetings between the four Chairs. In the first year of the new one Committee Structure (2021/22), there was five Committee meetings with 10 task group meetings. There is a cost to holding Committee meetings and so this has been reduced with the lower number of formal meetings.

Reduced number of reports for noting and more meaningful outcomes.

Although the cost of the O&S function should be considered, best value for residents should be a greater priority and the better use of Committee time key. A large number of reports deliberated by the old four committee structure were noted or requested that processes were “endorsed” in the final year of the old structure, 55 reports were considered in total with 20 that were purely noted or endorsed. In the first year of the new one Committee structure, a total of 19 items were considered with no reports purely for noting or endorsement. There were 2 recommendations requesting noting but these were in with others for those reports that did require outcomes from Members. The Annual report for 2021/22 showing all outcomes of reports considered is available to view here at item 12 [Agenda for Council on Tuesday, 17th May, 2022, 6.00 pm \(lichfielddc.gov.uk\)](#)

There be a robust selection of topics for O&S and a greater number of topics aligned with strategic plan

As part of the improvement package to the O&S function, a toolkit was introduced that detailed the function of O&S at Lichfield District Council as well as how Members could request topics be considered by the Committee. This was to ensure again that Committee’s time could be used most effectively and meaningful outcomes could be provided. The process has not been utilised as well as hoped however the current Chair has re-introduced the process at the first meeting of this year. The Committee is given the opportunity at each meeting to also raise topic suggestions to which the Chair considers whether there is a link to the Strategic Plan as well as impact to residents.

Chair and Vice Chair demonstrating skills needed

The Chair of Overview & Scrutiny is now appointed by the full Council to allow all to decide whether nominated Members have the right skills required for the enhanced role. Chairmanship training can also be provided.

Better questioning by members/less speech making

This is for the Committee to consider if the new O&S function has enabled effective questioning.

Track timelines from introduction to final approval, greater gaps will show earlier O&S involvement

This is for the Committee to consider if the new O&S function has enabled this.

Member Task Groups

- 3.2 As part of the changes, a greater use of Member Task Groups was agreed to provide the opportunity to conduct in-depth investigations into subjects.
- 3.3 These task groups could take a variety of forms from detailed policy development to a short, sharp concentrated focus on an issue. Membership could be from any Member of the Council to enable all that have an interest in the subject matter, an opportunity to be involved in the O&S function.
- 3.4 It was agreed in the change that Chairs of task groups should be trained or have previous experience in chairing meetings.
- 3.5 There have been five task groups since the new O&S function has come in however it should be noted that one of these were created under the previous regime. Further details of these task groups are below including membership. The membership of all current task groups needs to be reviewed as some individuals did not return following the local elections.

Lichfield City Masterplan

This group was created in October 2021 to consider strategies to aid the BRS site including the Car Parking strategy and Public Realm strategy. They have since been working with Officers and partners to consider wider city centre masterplanning, opportunities and constraints of the sites as well as consultation processes and responses.

Membership

Cllr Ball – Chair

Cllr Checkland

Cllr Cross

Cllr Ray

Vacant

Vacant

New Leisure Centre

This group was created before the new O&S function beginning in April 2020 following approval to build a new centre to have a much closer overview of the new build project as it progresses over time as well as to facilitate an agreed option for the new development and a mechanism for its delivery. They have agreed and recommended Stychbrook Park as the preferred site as well as investigated potential usage and leisure mix of the facility. They have also investigated development partners including site visits to other centres.

Membership

Vacant – Chair

Cllr Robertson

Cllr Silvester-Hall – removed as now a Cabinet member

Cllr Ray

Vacant

Vacant

Climate Change Emergency

This group was created in December 2021 following the Council's Climate Emergency Declaration. They have considered and the Council's own carbon reduction plan but are also looking at the Council can support residents and businesses in the district to reach the target of net zero carbon emissions by 2050.

Membership

Cllr Norman – Chair

Cllr Powell

Cllr Robertson

Vacant

Vacant

Kingham – external member

Taylor – external member

Dual Waste Recycling

This group to date met once in February 2022 to consider the communication strategy planned for the roll out of the dual recycling system. They reviewed leaflets and the utilisation of social media and were largely supportive of the plan but reserved the right for a follow up meeting if deemed necessary. Following the task group and its report to O&S Committee, its worked ceased.

Membership

Previous Cllr Matthews – Chair

Cllr Norman

Powell

Councillor Community Fund

This task group held one meeting in May 2022 to investigate the process and any issues with the Councillor Community Fund process during its trial. They fed back their findings to O&S Committee and it was not felt that another meeting was required however it has been agreed by the O&S Committee that a full review be conducted at the end of the year.

Membership

Previous Cllr Eagland – Chair

Warburton

- 3.6 As part of the new function process, every Task Group is required to have an agreed scope and all the scoping documents for the above groups are included at Appendix 1 (TO BE INSERTED)

Alternative Options	Members can choose to recommend to Council to revise and change the Committee function again.
Consultation	None for this report however the original review included Member Questionnaires and views of Officers. The Committee may wish to repeat this consultation for the full review.
Financial Implications	There is a financial cost to undertaking the O&S function as there is for all Committees. A full breakdown of this cost before changes to the one committee system was given to the then Member Task Group. It is intended to revisit and update the breakdown at the full review. However as mentioned, priority should be on best use of Committee time and level of outcome for the District. The Independent Remuneration Panel (IRP) did review the Special Responsibility Allowance in 2022 and make recommendations accordingly based on the new one Committee system which was agreed
Contribution to the Delivery of the Strategic Plan	A revised Committee Structure will ensure that LDC will be considered a good Council through ensuring effective use of its resources and decision making processes and supporting our strategic ambitions.
Equality, Diversity and Human Rights Implications	None from undertaking the review. Effective scrutiny will enable earlier and wider consideration of issues that will allow for more consultation and participation.
Crime & Safety Issues	None from undertaking the review.

Environmental Impact	It could be considered that there have been a number of positive impacts from changing the way that Committees are structured and conducted. Continuing with remote meetings in any capacity reduces carbon admissions from vehicles traveling to the Council House.
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GDPR/Privacy Impact Assessment	None from undertaking the review.
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	Risk Description	How We Manage It	Severity of Risk (RYG)
A	Outcomes of the review are not achievable within existing resources	The review has been led by ensuring all solutions are deliverable within existing resources	Green Impact med, likelihood low
B	Member recommendations do not ensure effective meetings	Options have been explored and best practice considered and developed into supporting processes. These need building into the constitution.	Yellow Impact high, likelihood low
C	Member recommendations undermine the purpose of meetings and our statutory obligations	Wide range of considered and relevant advice sought.	Green Impact high, likelihood low

Background documents
The Constitution https://democracy.lichfielddc.gov.uk/ieListDocuments.aspx?CId=190&Mid=304&Ver=4&info=1 https://democracy.lichfielddc.gov.uk/documents/s9618/Committee%20Review%20Strategic%20OS%20Report%20v3.pdf
Original Review Report to Cabinet (item 4) Agenda for Cabinet on Tuesday, 13th April, 2021, 6.00 pm (lichfielddc.gov.uk)

Relevant web links

Overview & Scrutiny Task Group

Subject scoping document

Review topic	Task Group membership	
Car Parking Strategy	Chair	CLlr Ball
	Members	CLlr Baker CLlr Checkland CLlr Cross CLlr Ray CLlr Warburton

Objectives	Desired outcomes
<p>Whilst going through the development and approval process of the initial draft Car Parking Strategy developed by 2020 Transportation Consultancy, Members set out that the document should be more strategic in nature with clear actions set out.</p> <p>To that end additional work has been undertaken by Officers, Project Board and Cabinet Members to develop a more structured final car parking strategy and action plan document, that aligns with the evidence base and recommendations included in the original consultant developed document, but includes more strategic actions that the Council intends to undertake to ensure the car parking provision within the city remains fit for purpose in the future.</p>	<p>The final Strategy and Action Plan sets out the following measures to be undertaken:</p> <ul style="list-style-type: none"> • Undertake an appraisal of the current situation in regards to public car parking provision and usage in Lichfield City Centre. • Carry out analysis of types of journeys being made by private cars using public car parks • Identify efficiencies that could be possible across the Council owned car parking stock and the impacts of such. • Make recommendations for demand management strategies. • Make recommendations as to the level and types of parking provision required and how these would be best served using existing and if necessary new provision. • Identify operational service improvements in managing and operating car parks to improve the customer experience (including the use of technologies to improve the ease of use of the service and installation of EV Charging Points). • Develop a detailed programme of projects that will deliver service improvements.

Terms of Reference
<p>At this meeting the group will be asked to consider the final Car Parking Strategy document and action plan against the original evidence base document that was created by 2020 Transportation Consultancy.</p> <p>The group will be asked to provide a response to Project Board who will seek approval to the final Car Parking Strategy at Cabinet on the 9th November 2021.</p>

Key issues	Risks
<ul style="list-style-type: none"> • Identification of efficiencies that could be possible across the Council owned car parking stock and the impacts of such. • Recommendations are deliverable • Recommendations as to the level and types of parking provision required are sufficient. • Service improvements that consider the customer experience are suitable. • Outcomes demonstrate value for money. 	<ul style="list-style-type: none"> • Time • Costs • Ability to demonstrate value for money

Timescale			
Start	October 2021	Finish	October 2021

Information requirements and sources	
Documents/evidence (what/why)	Final car parking strategy & Action Plan
Witnesses (who, why?)	Members
Consultation/research (what, why, who?)	Stakeholder engagement Public consultation process
Site visits (where, why, when?)	Consultants visited the city and the various car parks included within in the scope of works prior to the strategy being developed.

Officer support	
Lead Officers	Helen Bielby
Committee Clerk	Chris Lewis

Target body for findings/recommendations (e.g. Cabinet, Council etc.)
Cabinet 9 th November 2021.

All Task Group reports are to be submitted initially to the parent Overview and Scrutiny Committee(s)

Overview & Scrutiny Task Group

Subject scoping document

Review topic	Task Group membership	
Public Realm Strategy	Chair	Clr Ball
	Members	Clr Baker Clr Checkland Clr Cross Clr Ray Clr Warburton

Objectives	Desired outcomes
<p>The Council commissioned Gleeds to develop a detailed Public Realm Strategy. This document includes further development / critical appraisal of the ideas and ambitions for the Public Realm Priority Projects as outlined at high level within the city centre Masterplan, plus additional consideration/details of the wider aspirations for all public realm within the city centre.</p> <p>The Strategy includes outline costs of implementation and has developed an outline action plan or the delivery of the strategy which includes a realistically achievable programme plan.</p>	<p>The brief to the consultants required the following elements to be produced as part of the Public Realm Strategy:</p> <ul style="list-style-type: none"> • Production of a public realm analysis which includes a SWOT assessment of the value and purpose of key spaces and land uses; analysis of key views and landmarks; in-depth analysis of gateways and nodes. • Conduct an ownership audit and appraisal of existing street furniture including wayfinding, signage, lighting, trees and planting, surfacing materials and CCTV columns etc. • An “Access Strategy” detailing pedestrian, cycle and vehicle movements and links within, and to and from the fringe of, the city centre. This links to the wayfinding provision from these fringe areas and consideration of the pedestrianisation project being developed by Lichfield District Council/Staffordshire County Council. • The production of a draft Strategy, addressing issues raised as part of the public realm analysis, including baseline drawings, sketch proposals and a palette of materials. • The development of the 7 Priority Projects, if deemed feasible and appropriate, with design proposals. • Indicative costs of implementation. • Outline proposals and costs of future management and maintenance. • Development of an outline delivery strategy which includes a realistic and achievable programme plan and prioritisation of projects to be implemented. • Consultation with stakeholders, including public consultation events on the draft strategy. • Analysis of consultation and refinement of Public Realm Strategy for Council consideration/adoption.

Terms of Reference
<p>At this meeting the group will consider the draft public realm strategy, with particular focus on the proposed projects and recommendations that are emanating out of it.</p> <p>The group is asked to provide a response that will be recommended to Cabinet on the 9th November 2021.</p>

Key issues	Risks
<ul style="list-style-type: none"> • Recommendations are deliverable • Recommendations as to the level and types of public realm projects are fit for purpose and realistically deliverable. • Proposed improvements consider the customer experience are suitable. • Outcomes and outputs demonstrate value for money. 	<ul style="list-style-type: none"> • Time • Costs • Ability to demonstrate value for money

Timescale			
Start	October 2021	Finish	October 2021

Information requirements and sources	
Documents/evidence (what/why)	Draft Public Realm Strategy
Witnesses (who, why?)	Members
Consultation/research (what, why, who?)	Stakeholder engagement Public consultation process (consultation results are attached)
Site visits (where, why, when?)	Consultants visited the city and conducted site visits with the Major projects Development team in March and May 2021. Consultants were involved in walk “shops” of the city with stakeholders and members of the public during the public consultation process which took place in late June / early July 2021.

Officer support	
Lead Officers	Helen Bielby
Committee Clerk	Chris Lewis

Target body for findings/recommendations (e.g. Cabinet, Council etc.)
Cabinet 9 th November 2021.

All Task Group reports are to be submitted initially to the parent Overview and Scrutiny Committee(s)

**OVERVIEW AND SCRUTINY TASK GROUP
SUBJECT SCOPING DOCUMENT**

<p>Review Topic:</p> <p>Building a new leisure facility in Lichfield City.</p>	<p>Chair: Cllr Baker</p> <p>Members: Grange, Ray, Wilcox, Silvester-Hall, Robertson.</p>
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<p>Objectives:</p> <p>To have a much closer overview of the new build project as it progresses over time.</p> <p>To facilitate an agreed option for the new development and a mechanism for its delivery.</p>	<p>Desired Outcomes:</p> <p>An opportunity for Members to have insight to the project outside of the structured Overview and Scrutiny process.</p> <p>That Members are assured of an effective and deliverable project with appropriate decisions being supported and taken in a timely manner.</p>
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Terms of Reference:

See separate attachment – to be agreed

<p>Key Issues:</p> <ul style="list-style-type: none"> • Confidentiality • Future role and function of Leisure provision in Lichfield – its purpose • The extent of the development area • Capacity to serve particular identified needs • Delivery issues 	<p>Risks:</p> <ul style="list-style-type: none"> • Resources • Site options and feasibility • Stakeholder interests • Time pressures • Procurement regulations • Planning and related legislation
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Suggested Timescale

Start: April 2020
Finish: December 2024

Information Requirements and Sources:**Documents/Evidence:**

Background/contextual materials e.g. Local Plan requirements and related planning guidance such as Playing Pitch Strategy, Indoor Sports Facilities Plan, Sport England's Strategic Outcomes Planning Guidance Model, Physical Activity and Sports Strategy, Health and Wellbeing Strategy.

Officer Support:

Lead Officer: John Smith – Business Development Manager supported by Sarah Sleigh – Health and Wellbeing Manager

Committee Clerk: Christine Lewis

Target Body for Findings/Recommendations

The Task Group will act as a sub-Committee to the Leisure, Parks and Waste (Overview and Scrutiny) Committee.

It will make recommendations to the Project Board.

Overview & Scrutiny Task Group

Subject scoping document



Review topic	Task Group membership	
Climate Change Emergency	Chair	Cllr Norman
	Members	Cllr A. Little Cllr Powell Cllr Robertson Cllr Warburton

Objectives	Desired outcomes
To ensure the Council has a published and timetabled plan to get to net zero carbon emissions by 2050.	Reach this target, ideally before this date, both within the Council's own activities and in the wider district with the support of its residents.

Terms of Reference
Develop a plan in conjunction with Officers, Members and the public that has achievable goals and a clear timetable. To be limited to a task group timetable of six months followed by a yearly review by the Overview & Scrutiny Committee.

Key issues	Risks
Developing a realistic and workable plan. Work with local residents, organisations and businesses.	Not enough resources or political will to support it. Public support not achieved.

Timescale			
Start	13/12/2021	Finish	June 2022

Information requirements and sources	
Documents/evidence (what/why)	Council resolution Current carbon footprint
Witnesses (who, why?)	To be agreed
Consultation/research (what, why, who?)	Other Council's websites, Members, Officers. Public meeting early on.
Site visits (where, why, when?)	Non planned/anticipated.

Officer support	
Lead Officers	.
Committee Clerk	Chris Lewis

Target body for findings/recommendations (e.g. Cabinet, Council etc)
To be confirmed.

All Task Group reports are to be submitted initially to the parent Overview and Scrutiny Committee(s)

Overview & Scrutiny Task Group

Subject scoping document



Review topic	Task Group membership	
Dual Recycling	Chair	Tim Matthews
	Members	Steven Norman Alastair Little Harry Warburton Joseph Powell

Objectives	Desired outcomes
To review the communications strategy surrounding the rollout of Dual Recycling.	Ensure the communications strategy is sufficiently capable of securing the support and compliance of residents during this transition.

Terms of Reference
To review the communications regarding the Dual Waste Strategy to date and to suggest appropriate communication strategies to residents in the run up to the launch of the scheme and operation thereafter. Awaiting further information from the Cabinet Member.

Key issues	Risks
<ul style="list-style-type: none">• Achieving sufficient support and buy-in from local residents.• Explain why the change is required.• Ensuring dual stream is understood and utilised straight away by residents.	<ul style="list-style-type: none">• Public support not achieved.• Failure to adequately inform residents leads to incorrect use of the new system and contamination of materials.

Timescale

Start	Thursday 17 th February 2022	Finish	Monday 14 th March 2022
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Information requirements and sources

Documents/evidence (what/why)	<ul style="list-style-type: none">• Videos of existing Dual Stream services in Newcastle• Briefing Paper - Implementation of Dual Stream Recycling – Communications Planning
Witnesses (who, why?)	<ul style="list-style-type: none">• Ben Percival – Head of Operational Services (Responsibility for the Joint Waste Service in partnership with Tamworth Borough Council)• Nigel Harris – General Manager Joint Waste Service• Phillip Gillingham - Communications and Marketing Manager (Responsibility for the overall communications output of the authority)
Consultation/research (what, why, who?)	To be agreed
Site visits (where, why, when?)	Non planned/anticipated.

Officer support

Lead Officers	Phillip Gillingham, Nigel Harris, Ben Percival
O&S Committee Clerk	Chris Lewis
Task Group Clerk	Will Stevenson

Target body for findings/recommendations (e.g. Cabinet, Council etc)

All Task Group reports are to be submitted initially to the parent Overview and Scrutiny Committee(s)

Overview & Scrutiny Task Group

Subject scoping document



Review topic	Task Group membership	
Councillor Community Fund	Chair	
	Members	M. Wilcox, Warburton, Tranter, Robertson

Objectives	Desired outcomes
It has been requested that the group considers the governance and any risks of the current process.	To establish a robust and clear scheme for Members and residents

Terms of Reference
<p>To review what has gone well and not well with the first year of the scheme. To investigate what improvements can be made.</p> <p>To establish a robust scheme that Councillors and applicants are confident in participating in.</p>

Key issues	Risks
There have been issues experienced of over subscribing of funds.	Over burden of resource

Timescale	
Start	May 2022
Finish	

Information requirements and sources	
Documents/evidence (what/why)	Feedback from applicants and Councillors Usage figures
Witnesses (who, why?)	
Consultation/research (what, why, who?)	Cabinet Member
Site visits (where, why, when?)	None

Officer support	
Lead Officers	Susan Bamford
Committee Clerk	Chris Lewis

Target body for findings/recommendations (e.g. Cabinet, Council etc)

All Task Group reports are to be submitted initially to the parent Overview and Scrutiny Committee(s)

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Review of District and Parish Elections 2023

Date: 2 August 2023

Agenda Item:

Contact Officer: Christie Tims, Assistant Director Operations,
Regulation and Enforcement

Tel Number: 01543 308002

Email: Christie.tims@lichfielddc.gov.uk

Key Decision? No

Local Ward Members



OVERVIEW & SCRUTINY COMMITTEE

1. Executive Summary

- 1.1 Members have requested an opportunity to review the delivery and outcomes of the District and Parish Elections undertaken in May 2023.
- 1.2 Once every four years Lichfield District undertakes local elections to appoint 47 district members and appointments to the 27 Parish Councils that operate within the electoral area.
- 1.3 This year's election saw the introduction of Voter ID and was the first election under the new Chief Executive and Returning Officer, Simon Fletcher, and without a dedicated election team.
- 1.4 Despite many challenging aspects due to the changes made by the Elections Act 2022, the 2023 local the elections were delivered and no challenge to the results has been made.
- 1.5 Several learning points have been suggested in the post-election review and a formal lessons learned report is being collated to form the basis of an action plan to improve and streamline future elections.

2. Recommendations

- 2.1 Members note the content of the report and pass on observations and comments regarding the recent elections.
- 2.2 Members consider future activity and determine the requirement for future updates on this topic.

3. Background

- 3.1 Lichfield District is made up of electoral areas which form the basis of:
 - 22 District Council Wards (with between 1 and 3 seats in each, based on population numbers)
 - 27 Parish Councils (some of which are warded)
- 3.2 The Notice of Election was published on 20 March 2023, with 3 training sessions available to parish clerks and potential candidates on 14 and 24 March to advise on the nomination process and candidacy rules.
- 3.3 Nominations opened on 20 March and 356 were received by the team before the 4pm deadline on 4 April. Informal checks were available to nominees leading up to this every weekday between 10 and 4pm or by arrangement. However, due to the volume of nominations received in the last 72 hours of the nomination process, not all were double checked by a Deputy Returning Officer before the

deadline and 3 invalid nominations for parish appointments were received in this timescale which could not be accepted. (208 nominations for contested seats)

- 3.4 As a result of there being the same or fewer nominations than member vacancies one district member was elected at the close of nominations (Bourne Vale). 138 Parish members were also immediately elected, removing the need for elections in the following parish councils: Alrewas, Armitage with Handsacre, Burntwood Chase Terrace and Chasetown, Clifton Campville, Chasetown, Curborough and Elmhurst, Drayton Bassett, Edingale, Elford, Fisherwick, Fradley, Hammerwich, Harlaston, Hints and Canwell, Kings Bromley, Longdon, Mavesyn Ridware, Shenstone, Streethay, Swinfen and Packington, Thorpe Constantine, Wall, Weeford, Whittington, Wiggington & Hopwas (Hopwas).
- 3.5 Contests still took place in 21 district wards and 9 parish councils and wards, a total of 15 ballots. One Parish Council sadly received no nominations within the deadline (Farewell & Chorley) and resulted in a Notice of by election published on 10 May. Nomination papers were received for this parish council sufficient to appoint councillors without the need for a ballot in June.
- 3.6 Polling was undertaken on 4 May 2023 and an overall turnout of 25.3% was achieved, with turnout in each district poll varying from between 20% and up to 44% in some wards. Some parish ballots received a turnout of almost 49%, which is a slightly higher turnout than expected, given recent trends.
- 3.7 Thanks to an awareness campaign in the lead up to the elections the majority of voters who attended polling stations were fully prepared for Voter ID and there was only one reported incident by Presiding Officers.
- 3.8 Voter ID Evaluation Forms (VIDEF) were used by Presiding Officers on the day to capture details of those unable to vote due to Voter ID and out of 69,946 registered voters, 17,721 did so at a polling station, 24 were initially unable to vote, 16 of these later returned with an acceptable form of ID. In total, 8 voters were not issued with a ballot paper – a full breakdown of our statistics is available in the weblinks at the end of this report along with an interim analysis issued by the Electoral Commission on the national impact of Voter ID following the elections in May 2023.
- 3.9 This is the only local election at which VIDEF data is required to be captured. For the next two Parliamentary elections we will also need to complete VIDEF to provide statistics to the Electoral Commission and Secretary of State. Information collected via this process is published on our website, any other data we have at a more granular level can only be disclosed to the Secretary of State as detailed in the [Electoral Commission Guidance to Returning Officers](#)
- 3.10 Anecdotally, Voter ID had very little impact on polling day with the majority of voters aware of the requirement and prepared to show their ID. Of those reported as unable to vote at all (8) there was no noticeable trend that could be attributed from the information available/collected.
- 3.10 The additional training, processes and paperwork made recruitment to this election one of the main risks. This risk will be ongoing as many that did undertake roles this time found the new requirements complex. Options are being considered for future elections to ensure the process is as slick as possible for voters and staff working elections including:
- New polling system to replace paper documentation as polling stations – this will reduce paperwork at polling stations, ensure the ballot issuing process becomes fool proof, support effective record keeping and reduce receipting times
 - Increased recruitment, training and support for election and polling staff – with many new and less experienced poll staff it will be critical to provide sufficient support in advance of the next election for them to become more familiar with the technology and revised processes.
 - Revisions to staffing in the governance and election teams in the run up to elections – a wider pool of staff is required to deliver the elections effectively without staff having to work excessive hours. This will be particularly important in the next few elections which will run alongside normal council business and operations.

- Revisions to the Nominations process to ensure all forms received are signed off by a Returning Officer before the 4pm cut off. This will require all staff doing informal checks to be updating the system in real-time to ensure nominees are registered in the correct electoral area and not oversubscribed during the appointment.

3.11 Members should note the exceptional support provided by the governance, customer services, communications, HR and ICT teams and an army of volunteers from within the council and casual staff who went above and beyond to ensure delivery of this years' elections. Almost 400 staff in total worked to make it a success.

4. Future Milestones

- 4.1 We will of course implement any learning into future elections, the next planned election being 2 May 2024 PCC Elections.
- 4.2 Additional support has been appointed to lead the development of an action plan and carry out the revisions necessary in the next 6 months. Significantly more staff will need system access and training to support future elections from governance and other teams.
- 4.3 In October this year further aspects of the Elections Act will come into effect with the launch of a new online portal to allow absent voters to apply and renew their absent vote. Similar to the online Voter ID portal, this will replace the current paper-based system for absent vote applications and will require staff training to check and authorise postal and proxy votes. The new system will also issue a reminder to refresh absent voters' signature and vote every 3 years, rather than the current 5-year time limit.
- 4.4 In November 2023 the final aspects of the Elections Act 2022 will be made covering undue influence, intimidation, and digital imprints for those campaigning. By December this year we will also need to have removed all ineligible EU nationals from the revised electoral register.

Alternative Options	Not required for this report
Consultation	Not required for this report.
Financial Implications	<p>The Council has an election's earmarked reserve to fund the cost of District Council elections and 'smooth' the financial impact over the four-year election cycle.</p> <p>The balance of the District Council elections earmarked reserve at 1 April 2023 was £220,013.</p> <p>Any new electoral responsibilities required under law should be funded by New Burden's funding from the Government in order that the cost is not borne by local Taxpayers.</p> <p>Lichfield has received £19,443 to implement changes for May 2023, with a further £40,013 for costs incurred during the remainder of 2023/24. Further bids will be made to cover the additional costs caused by the new processes to mitigate the risks and ensure delivery.</p>
Approved by Section 151 Officer	Yes
Legal Implications	None.
Approved by Monitoring Officer	Yes
Contribution to the	To ensure we are an effective council

Delivery of the Strategic Plan	
Equality, Diversity and Human Rights Implications	Electoral changes were subject to EIA prior to implementation. Any action plan resulting from the lessons learned report will also be assessed for impact.
EIA logged by Equalities Officer	Equalities Officer confirmed not required.
Crime & Safety Issues	Not required for this report
Environmental Impact (including Climate Change and Biodiversity).	Not required for this report, though it is noted that elections are very paper heavy and efforts to streamline in future will assist in reducing carbon impact.
GDPR / Privacy Impact Assessment	Not required for this report

	Risk Description & Risk Owner	Original Score (RYG)	How We Manage It	Current Score (RYG)
A	That processes are not in place to deliver fair and transparent elections	Yellow (material) or as determined by the Likelihood Yellow and Impact Yellow	We will continue to review development of election processes and the changes to Electoral statutes.	Green (tolerable) as determined by the Likelihood Green and Impact Yellow
B	Staffing is not in place or sufficiently trained to deliver elections effectively	Yellow (material) or as determined by the Likelihood Yellow and Impact Yellow	New technology is being explored and plans developed to ensure staff are identified, trained and have the right support to deliver in future.	Green (tolerable) as determined by the Likelihood Green and Impact Yellow
C	New Burden's Funding does not reflect the full cost of implementing new electoral requirements and the cost is inadvertently borne by Local Taxpayers	Yellow (material) Likelihood: Yellow Impact: Yellow	The cost of implementing new Electoral Requirements is monitored and included on any financial returns to the Government. Where there are any 'caps' to funding levels that mean full cost reimbursement is not received, then this is communicated through consultations and other communication channels.	Yellow (material) Likelihood: Yellow Impact: Green

	<p>Background documents</p> <p>Agenda for Overview & Scrutiny Committee on Thursday, 16th March, 2023, 6.00 pm (lichfielddc.gov.uk)</p> <p>Agenda item - Voter ID Update (lichfielddc.gov.uk)</p>
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	<p>Relevant web links</p> <p>View the results of the local election – View the results (lichfielddc.gov.uk) (VIDEF data)</p> <p>Parish councils contact details (lichfielddc.gov.uk)</p> <p>Election results (lichfielddc.gov.uk)</p> <p>Voter ID at the May 2023 local elections in England: interim analysis Electoral Commission</p>
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OVERVIEW AND SCRUTINY COMMITTEE WORK PROGRAMME FOR 2023-24

AGENDA ITEM	DETAILS/REASONS	05.06.23	02.08.23	14.09.23	14.11.23	19.12.23	30.01.24	03.04.23	TASK GROUP REQUIRED YES/NO	OFFICER LEAD	MEMBER LEAD
Terms of Reference	To remind the Committee of the terms of reference and suggest any amendments	✓							NO	CLL	Cllr D. Pullen
Empty Homes Policy	From Forward Plan			✓						Lizzie Barton	Councillor A. Farrell
City Centre Pedestrianisation Trial	To review the trial so far including consultation responses specifically blue badge holders		✓							John Smith	Councillor D. Pullen
Planning Committee Protocol	To give views on the draft new protocol before consideration by the Planning Committee			✓						Lizzie Barton	Councillor A. Farrell
Councillor Community Fund	To undertake a full review of the scheme			✓					No		Cllr R. E Cox
Notes from Task groups	To receive the meeting notes from task group meetings	✓	✓	✓	✓	✓	✓	✓			
Review of the Overview & Scrutiny function at the Council	Following the move to a one O&S Committee system, it was agreed to review the effectiveness of the change.		✓					✓		Kerry Dove	
Review of Civic function matrix	To undertake a review of the current event matrix as devised by a previous Task Group and to consider any recommendation by the Independent Remuneration Panel			✓					Yes	Kerry Dove	Cllr D. Pullen

OVERVIEW AND SCRUTINY COMMITTEE WORK PROGRAMME FOR 2023-24

Funding for Infrastructure in Burntwood and rural areas							✓				
Cinema	To receive an update		✓							John Smith	Cllr D. Pullen
Medium Term Financial Strategy	To receive drafts and updates to the MTFS			✓	✓		✓			Anthony Thomas	Cllr R. Strachan
Financial Planning	To consider Service and Financial Planning Proposals					✓				Anthony Thomas	Cllr R. Strachan
Local Elections 2023 Review	To undertake a review of the Local Elections 2023 and its processes including Voter ID and accessibility.		✓							Christie Tims	Cllr D. Pullen
Briefing Papers											
Money Matters										Anthony Thomas	Cllr R. Strachan
Development Control Performance										Lizzie Barton	Cllr A. Farrell
Local Plan Update										Lizzie Barton	Cllr A. Farrell

Page 188